



The Village of **ESTERO**

COMPREHENSIVE PLAN

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INTRODUCTION

Purpose

This Comprehensive Plan provides the guiding principles for a balanced economic, social, physical, environmental, and fiscal future for Estero. This is a tool for members of the community, including Village officials, staff and residents, outlining a general framework for how the community is envisioned to grow. The Comprehensive Plan consists of separate Elements (or chapters) as prescribed in Chapter 163 of the Florida Statutes, specifically designed to address local circumstances. Within each Element are Goals, Objectives and Policies with increasing degrees of specificity to guide decision making related to all aspects of managing future growth in the Village. The content of the Plan is based on data, analysis and input from the public received during the preparation of the plan from September 2016 through December 2017.

What does the Comprehensive Plan achieve?

- Creates a collective vision to maintain the quality of life in Estero and to guide future growth and development
- Establishes the guidelines focused on ensuring Estero is a community with unique quality of life, distinct character, and diverse housing, economic, recreational, and social opportunities
- Seeks an interconnected transportation network that successfully links residential, commercial and recreational areas safely, economically and efficiently
- Establishes the importance of protecting and enhancing residential neighborhoods
- Ensures the public health, safety, and welfare of the community through appropriate coordination and management of infrastructure services
- Emphasizes a priority on both the protection of and access to Estero's natural resources
- Asserts the value placed on recreational opportunities and open spaces
- A commitment to annually analyze existing conditions and available finances to prioritize and implement capital improvements to maintain established level of service (LOS) standards
- Determines a commitment between the Village and the public school system to enhance and enrich both the community and the students of Estero
- Connects all elements together in a seamless document to clarify that no element stands independent from the others
- Emphasizes the priority given to community participation in the Plan's creation and advocates ongoing involvement from the public in its implementation

How was the Comprehensive Plan developed?

Since the Village's establishment on December 31, 2014, Estero has been working under the Lee County Plan during transition to the adoption of its own document. The process of the formation of the Comprehensive Plan involved the following key steps:

1. **LPA Kick Off Meeting:** The Local Planning Agency (LPA) of the Village is the Planning & Zoning Board.
2. **Data & Analysis Gathering:** Collection of data and analysis of Village land, population and infrastructure helped to identify issues and served as a foundation for public engagement and policy writing.
3. **Community Meeting - Informational Kick Off:** The first public session informed the public about the Comprehensive Planning process.
4. **Community Meeting - Vision Workshop:** This workshop engaged the citizenry in affirming the community's Vision.
5. **Policy writing & Workshops:** Each Element was presented to the LPA in initial concept with issues identified, and then presented in draft form in separate LPA Workshops and Village Council Workshops, providing opportunities for public input.
6. **LPA Public Hearing - Full Comprehensive Plan:** The full Comprehensive Plan was reviewed by the LPA for recommendation to Village Council, with opportunity for public input.
7. **Village Council Workshop & Public Hearing:** Village Council considered the full Comprehensive Plan in a workshop, and then in a public hearing, both with opportunity for public input. At the public hearing, Village Council approved transmittal to the Florida Department of Economic Opportunity (DEO).
8. **Village Council Public Hearing:** After addressing DEO comments, Village Council adopted the Comprehensive Plan.

How do I read the Elements?

Each Element is broken down into Goals, Objectives and Policies. Goals are overarching statements within the Elements, labeled with an abbreviation and number.

*For example, the Future Land Use Goal is **FLU-1**.*

This hierarchy continues to a series of Objectives that achieve the defined Goal. They are also numbered and titled.

*For example, the first Objective in the Future Land Use Element is **FLU-1.1**, the second is **FLU-1.2**, and so on.*

Each Objective is supported by a series of Policies that vary in level of detail, length and number depending on the Objective.

*For example, under the **Objective FLU-1.3 Specific Use Considerations** there are ten policies, labeled as **FLU-1.3.1** to **FLU-1.3.10**.*

Maps are identified in the text and labeled with an abbreviation and a number.

*For example, the Future Land Use Map is **Map FLU-1**.*

COMMUNITY PROFILE

The name Estero is Spanish for estuary – where the river meets the sea – an appropriate name for a Village situated along a river that meanders into the pristine Estero Bay. While Estero was most recently incorporated in 2014, it is not the first incorporation. The original Town of Estero was founded in 1904, but it was short-lived and was abolished in 1907. Many significant historic structures in the Village today date back to this time period, including structures maintained at the Koreshan State Historic Site.

A strong appreciation for historic character and conservation of natural resources are foundations that shaped the Estero area as it grew to become home to over 30,000 residents. During the growth period of the 2000s, it was recognized by residents that the maintenance of the area's special aesthetic quality and identity called for special action. In 2013, incorporation efforts began. Thirty-three community meetings were held and over 3,000 Estero residents attended two Town Hall meetings. These efforts resulted in 10,238 petitions signed by residents indicating their wish to have an incorporation referendum on the ballot.

The Estero incorporation referendum on the November 4, 2014 ballot passed with 86 percent of votes. The Village of Estero was officially incorporated on December 31, 2014. The Village is led by a seven-member elected Council and Village Manager.

The Village of Estero had a permanent population of ~~30,851~~34,631 in ~~2016~~2018, with a peak seasonal population of ~~46,091~~51,738. Approximately half of the ~~ea~~at population is aged 62 years or older, which is a significantly higher proportion than Lee County (30%) and the State of Florida (22%). A majority of the available dwelling units are single family (59%), relatively new (84% built between 1990 and 2009), and owner occupied (84%).

VISION

The Village of Estero desires to be a community with a high quality of life and expanding economic opportunities which embraces its historic heritage and protects the environment, while carefully planning for future development and utilizing the proximity to Florida Gulf Coast University and the Southwest Florida International Airport as an asset and a positive economic driver. The Village of Estero's growth will be planned with strong neighborhoods, diverse economic generators, interconnected mixed-use centers, varied parks, public spaces, recreational facilities, and unique natural environments that fosters belonging and creates a sense of place. The Village strives to be a highly valued place to live, work, and visit because of development standards and design guidelines that promote: 1) desirable neighborhoods and public amenities; 2) vibrant economic centers; 3) attractive landscaping, streetscaping, lighting, signage, and architectural features; and 4) an interconnected transportation network.

The Village is primarily made up of residential neighborhoods, and the Village will continue to place priority on maintaining the quality of life that the residents enjoy in the various neighborhoods of Estero. Where feasible, the Village will provide for the development of walkable mixed-use town centers and economic areas featuring diverse housing options; government offices and public facilities; medical facilities; employment centers; public gathering places, parks, outdoor plazas, and other public spaces; greenway trails and pathways; and public access to the community's natural resources through comprehensive plan policies and LDC regulations that support the Village's distinct community character and the following community priorities:

- A. Supporting the development of a village center to unify the community;
- B. Improving the connectivity between Estero's residential neighborhoods, economic areas, civic uses, parks, and recreational facilities;
- C. Diversifying the community's economic base and employment opportunities
- D. Encouraging the development of targeted industry clusters; particularly health industries, technology, professional services and businesses, and research and development;
- E. Expanding multi-modal transportation options through improved pedestrian access, bikeways, transit service, and rail opportunities;
- F. Improving access to the community's blueways - particularly the Estero river, greenway trails, other open spaces;
- G. Promoting the community's cultural and historic resources; public spaces, parks, and recreational facilities; and other community amenities;

- H. Providing ample and varied public gathering places, including, without limitation, parks, plazas, sidewalks, benches, restaurants, shops, civic spaces, green spaces, community recreation centers, and other recreational facilities;
- I. Maintaining a unified and consistent aesthetic/visual quality and sustainability in landscaping, architecture, lighting, and signage in all commercial and mixed-use developments; and
- J. Promoting and incentivizing private investment within mixed-use centers and economic areas.

FUTURE LAND USE

the Essence

The future land use element focuses on ensuring that Estero is a community with unique quality of life, distinct character, and diverse housing, economic, recreational, and social opportunities. Thoughtful and careful planning of the physical environment in harmony with the natural environment will help maintain the essence of Estero, the *Village with a Vision*.



Balance

The balance of attractive natural and built environments is a foundation of the community that contributes to the high quality of life that residents enjoy.

Neighborhoods

Estero is a Village of neighborhoods... some large, some small, some more established, some newer. Ensuring compatibility of new development to protect the integrity of existing and future residential neighborhoods is a key to sustaining the quality of life.

Aesthetics

It is apparent that one has arrived in the Village by the architecture and landscaping that gives Estero its look and feel. The distinctively designed and developed built environment will continue to define Estero.

Participation

Public participation has been a strength of Estero, even before its incorporation. The emphasis remains on the importance of an inclusive, informative community planning process.

Sense of Place

It is important to address the needs of residents with places that are comfortable, vibrant and accessible. To ensure that future development and redevelopment meets the needs of residents, mixing of uses and placemaking strategies will be used in new development as appropriate.

There are 30,945 people who call Estero home as of April 2017 per the Bureau of Economic and Business Research at the University of Florida. Many of Estero's residents will say their favorite qualities are the location, natural resources and high-quality appearance of the built environment. The Future Land Use Element contains the goals, objectives and policies that help sustain and further these community attributes. This is done through:

Managing the Type, Location, and Intensity of Future Land Uses

The Future Land Use Map depicts the distribution, location, and extent of future land uses by type, density, and intensity to provide a balance of uses and ensure that Estero has a diverse residential and economic base while meeting the needs of the people who live, work, and play within the community.

Recognition of Estero's Historic Resources

The Village is rooted in a rich history. The Koreshan State Historic Site and the landmark vegetation, buildings and streets of the surrounding Old Estero area are recognized as assets that help the community maintain a connection to this history.

Opportunities for Public Participation in the Land Development Approval Process

Estero has a strong tradition of public participation, even before its incorporation as a Village. This is an important policy that will continue to foster a true sense of place in for the community.

Corridor Planning

Estero's residents and visitors primarily experience Estero along its streets and roadways. It is important to manage the appearance and function of the Village's corridors by coordinating transportation and land use considerations with emphasis on safety to encourage more multimodal use and human scale activity along the corridors.

Community Design Sub-Element:

Design standards for mixed used, commercial and residential developments

Design and compatibility criteria are established for different types of development with incentives to encourage the appropriate mixing of residential and commercial uses. These standards ensure new developments and redevelopments contribute to the attractiveness of the community, enhance quality of life, and foster a unique sense of place.

FLU-1

GOAL

Promote the development of Estero as a community with a unique quality of life, distinct character, and diverse housing, economic, recreational, and social opportunities by protecting the natural resources, environment, and lifestyle; establishing aesthetic and design requirements; managing the type, location, quality, design, density and intensity of future land uses; providing opportunities for public participation in the land development approval process; and promoting a true sense of place in Estero.

COMMUNITY CHARACTER

FLU-1.1

OBJECTIVE

Estero residents enjoy a high quality of life characterized by a balance of attractive natural environments, lasting residential neighborhoods, and a distinctively designed and developed built environment. These are the characteristics that make Estero unique and will guide the Village's future planning practices. The Village's implementation of planning and development practices shall contribute to a visually attractive community, an enhanced quality of life, and continue to foster this unique sense of place.



Image by Johnson Engineering, Inc.

POLICIES

FLU-1.1.1 Support the unique character and quality of life within the Estero community by managing growth and development and by maintaining and executing comprehensive plan policies, Land Development Code (LDC) regulations, and other planning tools that:

- A. Implement and maintain high quality development standards for architecture, landscape architecture, hardscaping, site planning, landscaping and buffering, signage, lighting designs and visual appearance of developments, transportation facilities, pedestrian circulation and community amenities including public spaces and parks within mixed use and non-residential developments;
- B. Promote the use of low impact design, sustainable energy, water, and other environmental features;
- C. Encourage higher density, compact mixed-use development in designated locations as a means of increasing internal capture and reducing overall vehicle miles travelled;
- D. Promote targeted industries in appropriate areas of Estero—e.g.: healthcare, arts and culture, technology, and Class A office facilities;

- E. Encourage the redevelopment of underutilized commercial and residential lands for older properties seeking to comply with current comprehensive plan policies and LDC standards; and
- F. Facilitate public participation in the land development approval process by having the owner or agent applying for Planned Developments, Rezoning, Variances, Special Exceptions, Plan Amendments, and other amendments and Development Orders, within the Village, participate in a public information meeting pursuant to adopted Village regulations and policies.

FUTURE LAND USE MAP

OBJECTIVE

Maintain and enforce a Future Land Use Map showing the proposed distribution, location, and extent of future land uses by type, density, and intensity to provide a balance of uses that foster vibrant, viable neighborhoods and economic development opportunities, protect natural and man-made resources, provide essential services in a cost-effective manner, maintain community character, and discourage urban sprawl.

FLU-1.2

POLICIES **FLU-1.2.1 FUTURE LAND USE CATEGORIES.** The Village shall designate on the Future Land Use Map (see Map FLU-1) use categories of varying densities and intensities to provide for a full range of land use activities appropriate to the character of the Village's neighborhoods. These designations are based upon environmental conditions, historic and developing growth patterns, and existing or future availability of public facilities and services. Village future land use categories are summarized as follow:

Category	General Uses Allowed <i>See policies for full listing of uses</i>	Max. Density/Intensity	Standards
Conservation	Preserves, mitigation areas and banks, natural resource based parks, and water conservation	n/a	
Wetlands	Very limited residential and recreation, open space and conservation	1 du / 20 acres	Avoid adversely affecting the ecological functions of wetlands
Public Parks and Recreation	Preserved land, passive recreation, active recreation, community centers	n/a <u>1 FAR</u>	

FUTURE LAND USE

the Essence

Category	General Uses Allowed <i>See policies for full listing of uses</i>	Max. Density/Intensity	Standards
Public Facilities	<u>Public schools</u>	n/a	
	<u>Public schools</u> , <u>libraries</u> , post office, fire station, utility sites, and other governmental facilities	<u>n/a</u> <u>1 FAR</u>	
Village Neighborhood 1	Schools, <u>churches</u>	n/a	Traffic shall not travel through significantly lower density or intensity areas
	<u>Churches</u>	<u>1 FAR</u>	
	<u>Assisted living facilities</u> , <u>public and quasi-public uses</u>	<u>1 FAR</u>	
	<u>Assisted living facilities</u>	<u>1-4 du/ac, subject to conversion standards in the LDC</u>	
	Residential	1 – 4 du / gross residential acre	Mixed use or single use commercial allowed subject to compatibility standards and public hearing
	Minor commercial	<u>1 FAR or up to 30,000 s.f., whichever is less</u>	
	Neighborhood commercial	30,000 – 100,000 s.f. Per FLU-1.5	
Village Neighborhood 2	Schools, <u>churches</u>	n/a	Traffic shall not travel through significantly lower density or intensity areas
	<u>Churches</u>	<u>1 FAR</u>	
	<u>Assisted living facilities</u> , <u>public and quasi-public uses</u>	<u>1 FAR</u>	
	<u>Assisted living facilities</u>	<u>1-6 du/ac, subject to conversion standards in the LDC</u>	
	Residential	1 – 6 du / gross residential acre	Mixed use or single use commercial allowed subject to compatibility standards and public hearing
	Minor commercial	<u>1 FAR</u> ; Up to 30,000 s.f.	
	Neighborhood commercial	<u>1 FAR</u> ; 30,000 – 100,000 s.f. Per FLU-1.5	

FUTURE LAND USE

the Essence

Category	General Uses Allowed <i>See policies for full listing of uses</i>	Max. Density/Intensity	Standards
Transitional Mixed Use *	Schools, churches	n/a	Traffic shall not travel through significantly lower density or intensity areas
	<u>Churches</u>	<u>1 FAR</u>	
	Assisted living facilities, public and quasi-public uses	<u>1 FAR</u>	
	<u>Assisted living facilities</u>	<u>1-6 du/ac, subject to conversion standards in the LDC</u>	
	Regional, community, neighborhood and minor commercial, hotel and office	Up to 1 million s.f. Per FLU-1.5	Mixed use or single use commercial allowed subject to compatibility standards and public hearing
	Residential	1 – 6 du / gross residential acre	
	Horizontal mixed use	<u>1 FAR for non-residential uses</u> 1 – 6 du / gross residential acre	
	Integrated horizontal mixed use with placemaking	<u>1 FAR for non-residential uses</u> 1 – 10 du / gross residential acre, bonus up to 14	
	Vertical mixed use	<u>1 FAR for non-residential uses</u> 1 – 16 du / gross acre including non-residential land, bonus up to 20	
Urban Commercial	Schools, churches	n/a	Traffic shall not travel through significantly lower density or intensity areas
	<u>Churches</u>	<u>1 FAR</u>	
	Assisted living facilities, p Public and quasi-public uses	<u>1 FAR</u>	
	<u>Assisted living facilities</u>	<u>1-6 du/ac, subject to conversion standards in the LDC</u>	

FUTURE LAND USE

the Essence

Category	General Uses Allowed <i>See policies for full listing of uses</i>	Max. Density/Intensity	Standards
	Community, neighborhood and minor commercial, hotel and office	<u>1 FAR</u> ; Up to 400,000 s.f. Per FLU-1.5	Single use commercial subject to compatibility standards
	Residential	1 – 6 du / gross residential acre	Limited to parcels of 5 or more acres in size or parcels subject to an approved and effective DRI that designates residential development as allowable on the parcel
	Integrated horizontal mixed use	<u>1 FAR for non-residential uses</u> 1 – 6 du / gross residential acre	Mixed use allowed subject to compatibility standards and public hearing
	Vertical mixed use	<u>1 FAR for non-residential uses</u> 1 – 6 du / acre based on gross acreage including non-residential land	
Village Center <u>**</u>	Schools, churches	n/a	Traffic shall not travel through significantly lower density or intensity areas
	<u>Churches</u>	<u>2 FAR</u>	
	Assisted living facilities, p Public and quasi-public uses	<u>2 FAR</u>	
	<u>Assisted living facilities</u>	<u>1-6 du/ac, subject to conversion standards in the LDC</u>	
	Housing, employment, hospital, shopping, office, hotel, recreation, park and civic uses	<u>2 FAR for non-residential uses</u> ; Standard density range: up to 6 du/gross residential acre Tier 1: up to 6 du/gross residential acre, Incentives up to 9 Tier 2: up to 10 du/gross acre including non-residential land, Incentives up to 14 Tier 3: up to 15 du/gross acre including non-residential land, Incentives up to 20	

Category	General Uses Allowed <i>See policies for full listing of uses</i>	Max. Density/Intensity	Standards
		Tier 4: up to 21 du/gross acre including non-residential land, Incentives up to 27	
<p>* See policy FLU-1.2.8 B</p> <p>** See policy FLU-1.2.10 E</p>			

FLU-1.2.2 CONSERVATION. The Conservation Lands include uplands and wetlands that are owned and used for long range conservation purposes. Conservation Lands may include such uses as wildlife preserves; wetland and upland mitigation areas and banks; natural resource based parks; ancillary uses for environmental research and education, historic and cultural preservation, and natural resource based parks (such as signage, parking facilities, caretaker quarters, interpretive kiosks, research centers, and quarters and other associated support services); and water conservation lands such as aquifer recharge areas, flowways, flood prone areas, and well fields.

Preserving the integrity of these areas for conservation enhances the aesthetics and quality of life for Village residents and tourists, provides a degree of natural protection against storms and flooding, helps maintain air and water quality, promotes marine and wildlife diversification and productivity, and promotes soil stabilization.

FLU-1.2.3 WETLANDS. Wetlands are those lands that are identified as Wetlands on the Future Land Use Map in accordance with Florida Statute Section 373.019(27). If a Wetlands boundary on the Future Land Use Map is incorrect due to a clear factual error, or if an exact boundary is established per jurisdictional determination or Florida Statute Section 373.019, an administrative process may be used to establish the precise boundary of the Wetland.

- A. Uses: Permitted land uses in Wetlands consist of very low density residential uses and recreational uses that will not adversely affect the ecological functions of wetlands, open space, and conservation that are compatible with wetland functions.
- B. Development type: Development in Wetlands must not adversely affect the ecological functions of wetlands.
- C. Density: The maximum density is one dwelling unit per twenty acres (1 du/20 acre).
 1. To encourage the preservation of lands designated Wetlands, any Wetlands area that is preserved may be calculated to yield dwellings at three-fourths (or 75%) of the standard density as allowed for adjacent land under common ownership designated Village Neighborhood 1, Village Neighborhood 2, Transitional Mixed Use, or Urban Commercial (formerly Intensive Development, Central Urban, Urban Community, Suburban, and Outlying Suburban areas). The number of dwellings calculated as yield from the preserved Wetlands may be transferred and incorporated in the development of the adjacent land under common ownership.

2. Any Wetlands designated area that is impacted will be calculated at the standard Wetlands density of 1 dwelling unit per 20 acres.

FLU-1.2.4 PUBLIC PARKS AND RECREATION. The Public Parks and Recreation areas include publicly owned or publicly accessible land for current or future active or passive recreational uses. Public Parks and Recreation areas are located where park facilities are established or planned, including state, county and local parks and recreation areas.

- A. Uses: Land uses range from preserved lands with passive recreation to community centers with active recreation uses, along with ancillary or supporting uses necessary for park and recreation operations serving the public.
- B. Development type: Public Parks and Recreation areas may include development of public parks, playgrounds, water access, and other active recreation facilities, as well as preservation, walking and bicycling trails, picnicking and boardwalks to provide passive recreation.

C. Intensity: Development must not exceed 1 FAR.

FLU-1.2.5 PUBLIC FACILITIES. Public Facilities areas include the publicly owned lands within the Village that are developed or anticipated for development to serve public purposes such as public schools, library, post office, fire station, utility sites, and other governmental facilities. The allowable uses within these areas are determined by the entity owning each such parcel in coordination with the Village's zoning and permitting provisions for these facilities. Development (except schools) must not exceed 1 FAR.

FLU 1.2.6 VILLAGE NEIGHBORHOOD 1. The Village Neighborhood 1 areas are characterized by predominantly residential uses of lower densities with the potential for commercial uses of a minor commercial or neighborhood center scale. Some, but not all, of the requisite infrastructure needed for development is generally planned or in place. These areas were generally designated Rural, Sub-Outlying Suburban, Outlying Suburban, or Suburban in the Village's Transitional Comprehensive Plan. These areas have been developed or planned with existing or emerging residential neighborhoods. Golf courses constitute the predominant open space in many of these neighborhoods, and gross densities range from one dwelling unit per acre (1 du/acre) to four dwelling units per acre (4 du/acre). Village Neighborhood 1 designated areas are either on the fringe of natural resources, or in proximity to urban commercial or transitional mixed-use areas. Examples include Wildcat Run, West Bay Club, The Estates at Estero River, the Brooks and Pelican Landing.

- A. Uses: Predominant land uses in Village Neighborhood 1 will be lower density residential uses with the potential to allow minor commercial or limited neighborhood center scale commercial, assisted living facilities, public and quasi-public uses, in each case through a public hearing process. Higher densities greater than 4 du/ac, commercial development greater than neighborhood centers, and industrial land uses are not permitted.

- B. Development type: Future development in this category is encouraged to be developed in a way that maintains and enhances a limited mix of residential unit types (multifamily, townhome, mid-rise, villas, and single family) while accommodating neighborhood oriented commercial uses such as eating places, personal and convenience services, and limited office when approved through a public hearing process. Such commercial uses may be developed as single use buildings or centers or as mixed-use, subject to compatibility standards described in the Community Design Sub-Element.
- C. Density/Intensity: The standard density range is from one dwelling unit per acre (1 du/acre) to four dwelling units per acre (4 du/acre). Bonus densities are not allowed. Commercial development must not exceed 1 FAR and must not exceed neighborhood center criteria of 30,000 – 100,000 square feet as described in Objective FLU-1.5.

FLU-1.2.7 VILLAGE NEIGHBORHOOD 2. The Village Neighborhood 2 areas are characterized by predominantly residential uses of moderate densities with the potential for commercial uses of minor commercial or neighborhood center scale. These areas have urban services available and are typically smaller, more compact sites that were designated Urban Community or Suburban (and not within the Mixed-Use Overlay) in the Village's Transitional Comprehensive Plan. These areas have been developed with residential communities with gross densities ranging from over four dwelling units per acre (4 du/acre) to a maximum of ten dwelling units per acre (10 du/acre). Included among them, for example, are Coconut Shores, Fountain Lakes, Corkscrew Woodlands, Copper Oaks, Breckenridge, and Villagio.

- A. Uses: Predominant land uses in Village Neighborhood 2 will be moderate density residential uses with the potential to allow minor commercial or neighborhood center scale commercial, assisted living facilities, and public and quasi-public uses, in each case through a public hearing process. Higher densities greater than 6 du/ac, commercial development greater than neighborhood centers, and industrial land uses are not permitted.
- B. Development type: Future development in this category is encouraged to be developed in a way that maintains and enhances the variety and mix of residential unit types (multifamily, townhome, mid-rise, villas, and single family) while accommodating neighborhood oriented uses such as eating places, personal and convenience services, and office when approved through a public hearing process. Such commercial uses may be developed as single use buildings or centers or as mixed-use, subject to compatibility standards described in the Community Design Sub-Element.
- C. Density/Intensity: Standard density ranges from one dwelling unit per acre (1 du/acre) to six dwelling units per acre (6 du/acre). Commercial development must not exceed 1 FAR and must not exceed neighborhood center criteria of 30,000 to 100,000 square feet as described in Objective FLU-1.5.

FLU-1.2.8 TRANSITIONAL MIXED USE. Transitional Mixed Use areas are characterized by primarily existing or emerging developments where the Village's largest and most intense commercial and multifamily residential developments are or will be concentrated, including some areas that currently have some degree of mixed use. Urban services are in place or readily expandable to support moderately intense levels of mixed commercial and residential development. These areas were typically designated Urban Community, General Interchange, or Suburban and within the Mixed Use Overlay per the Village's Transitional Comprehensive Plan. Transitional Mixed Use areas are generally the north Estero area near intersections of US 41 with Broadway and Estero Parkway, Coconut Point DRI, and the four quadrants of the I-75 and Corkscrew Road interchange. These areas are located in close proximity to: public transit routes; education facilities; recreation opportunities; and existing residential, shopping and employment centers and are generally accessible to the surrounding neighborhoods, serving as focal points or community centers where activity is concentrated.

- A. **Uses:** A broad mix of uses, subject to compatibility standards of the Community Design Sub-Element, is allowed in the Transitional Mixed Use areas to foster the conveniences and efficiencies of live/work/play environments, including regional, community and neighborhood scale commercial including shopping, restaurant, entertainment and office, low to moderate density residential, parks and recreation. Assisted living facilities, and public and quasi-public uses are allowed in the Transitional Mixed Use future land use category.
- B. **Development type, Density and Intensity:** Development types and corresponding maximum densities and intensities allowed in the Transitional Mixed Use areas are outlined in the following table:

Transitional Mixed Use Category Development Types, Density and Intensity			
Development Types	Max. Standard Density <u>**</u>	Max. Bonus Density	Max. Intensity
Single use* & Horizontal mixed use* subject to compatibility standards *not allowed for substantial changes to existing approvals (10% or more increase to approved density or intensity)	1 du/ acre – 6 du/acre based on gross residential acreage for horizontal mixed use projects	n/a	<u>1 FAR; and</u> Neighborhood, community or regional scale commercial per FLU-1.5
Integrated horizontal mixed use subject to compatibility standards	1 du/acre – 10 du/acre based on gross residential acreage	Up to 14 du/acre based on gross	<u>1 FAR; and</u> Neighborhood, community or regional

		residential acreage	scale commercial per FLU-1.5
Vertical mixed use subject to compatibility standards	1 du/acre – 16 du/acre based on gross acreage including non-residential land	Up to 20 du/acre based on gross acreage including non- residential land	<u>1 FAR; and</u> Neighborhood, community or regional scale commercial per FLU-1.5
<u>** Bonus densities in the Transitional Mixed Use Land Use Category shall be determined through the planned development process for developer contributions of land or funds for the Village to purchase lands for public open space, recreational use or environmentally sensitive lands. The Village land development regulations shall establish the number of units available for such contributions and any additional compatibility criteria for planned development making use of the bonus densities</u>			

FLU-1.2.9 URBAN COMMERCIAL. The main purpose of the Urban Commercial designation is to foster development of unique destinations rather than a strip development pattern. Urban Commercial areas are characterized by primarily existing or emerging developments along the Village's corridors where primarily single use retail, restaurant office, assisted living uses are concentrated. These properties are generally smaller in size (less than 10 acres) and serve nearby residential neighborhoods and consumers from outside Estero with daily and occasional needs for goods and services. Urban services are in place or readily expandable to support moderately intense levels of commercial development. Residential uses are also allowable, however they are not incentivized. These areas were typically designated Urban Community or Suburban and within the Mixed-Use Overlay per the Village's Transitional Comprehensive Plan. Urban Commercial areas are generally the individual development sites along major corridors of U.S. 41, Corkscrew Road, and Three Oaks Parkway. These areas are located in close proximity to: public transit routes; education facilities; recreation opportunities; and existing residential, shopping and employment centers.

- A. Uses: A broad mix of commercial uses is allowed in the Urban Commercial areas to foster convenience and efficiency for a broader live/work/play environment, including neighborhood and community scale commercial including shopping, restaurant, hotel and office, all subject to the compatibility standards of FLU-1.10. Assisted living facilities, public and quasi-public uses are allowed in the Urban Commercial future land use category. Residential development is allowed subject to compatibility standards, and limited to parcels of 5 or more acres in size or parcels subject to an approved and effective Development of Regional Impact (DRI) that designates residential development as allowable on the parcel.
- B. Development type, Density and Intensity: Development types and corresponding maximum allowed densities and intensities in the Urban Commercial areas are outlined in the following table:

Urban Commercial Category Development Types, Density and Intensity			
Development Types	Standard Density	Bonus Density	Intensity
Single use commercial subject to compatibility standards	n/a	n/a	<u>1 FAR; and</u> Neighborhood or community scale commercial per FLU-1.5
Residential Limited to parcels of 5 or more acres in size or parcels subject to an approved and effective DRI that designates residential development as allowable on the parcel	1 du/acre – 6 du/acre based on gross residential acreage	n/a	n/a
Integrated horizontal mixed use subject to compatibility standards	1 du/acre – 6 du/acre based on gross residential acreage	n/a	<u>1 FAR; and</u> Minor, Neighborhood or Community scale commercial per FLU-1.5
Vertical mixed use subject to compatibility standards	1 du/acre – 6 du/acre based on gross acreage including non-residential land		<u>1 FAR; and</u> Minor, Neighborhood or Community scale commercial per FLU-1.5
Assisted Living Facilities	1-6 du/ac, subject to conversion standards in the Land Development Code	n/a	

FLU-1.2.10 VILLAGE CENTER. The Village Center area is characterized by primarily undeveloped or underdeveloped land lying near US 41 in the heart of the Village of Estero. The Village Center is intended to improve the quality of life for Estero's residents and visitors by providing a variety of neighborhood and housing types and more diverse economic activity in the heart of Estero. The specific goals of the Village Center include creating: socially vital centers supportive of business both big and small, a central location for a hospital with full range of medical services, neighborhoods and streets that are safe and attractive for walking and bicycling, the preservation of community history, and the protection of the environment, particularly along the Estero River. Urban services are in place or readily expandable to support relatively intense commercial and residential development in the Village Center. The Village Center area has some of the larger

undeveloped parcels of land left in the Village in close proximity to Village Neighborhoods, Transitional Mixed-Use areas, major attractions of the County and State parks and Estero River, and major corridors of the CSX Railway, US 41 and Corkscrew Road.

- A. Uses: Uses in the Village Center include housing, employment, hospital, shopping, office, hotel, recreation, park and civic uses. Public schools, assisted living, public and quasi-public uses are allowed in the Village Center future land use category.
- B. Development type: Future development in this category is encouraged to be interconnected neighborhoods and mixed-use of low to midrise residential, commercial and mixed use building types in an urban form and including passive and active recreation facilities.
- C. Density/Intensity: Landowners in the Village Center Area may develop within the standard density range with one dwelling unit per acre (1 du/acre) to six dwelling units per acre (6 du/acre); however the Village encourages land to be developed or redeveloped with a greater mix of uses and higher densities per a tiered system described further below, with up to a maximum of 27 dwelling units per acre when the highest degree of walkable mixed-use pattern is achieved, subject to the public hearing process. Commercial development must not exceed 2 FAR.
- D. Implementation: The Village Center is implemented through LDC standards, plans, and incentives that address the community's need for a central civic and economic core that is connected to surrounding residential neighborhoods, commercial areas, the Estero River, the Old Estero area, the Estero Community Park, and other parks and recreational facilities.
 1. The Land Development Code establishes the Estero Planned Development Zoning District which contains tiered standards that apply to the Village Center Area and which may include sub-districts which may have specific policies applying therein. Rezoning to the Estero Planned Development Zoning District must be sought to take advantage of the new tiered standards and densities with respect to specific development tracts. The Village's intention is to use this new zoning district whenever increases in density and intensity are requested in the Village Center Area (as "increase in intensity" is defined in this Plan). The Village will coordinate with FDOT in the review of any new "Village Center" land development or redevelopment project to assess potential impacts of density increases on State and SIS transportation facilities, such as US 41 and 1-75.
 2. The Land Development Code provisions that implement the Village Center future land use category consider such reasonable guidelines as are necessary in order to foster predictable built results and higher quality public spaces by using physical form (rather than separation of uses) as the organizing principle for achieving such objectives. Such guidelines may consider designating locations where different building form standards apply, the relationship of buildings to the public space, public standards for such elements in the public space as sidewalks, travel lanes, on-

street parking, street trees, street furniture, and other aspects of the urban built environment that may be applicable to foster interconnection, social vitality and walkability in the Village Center Area. The Land Development Code provisions may also consider other alternative types of reasonable guidelines that may accomplish such goals in a different or complementary manner.

3. Properties in the Village Center Area which have vested rights under the law may proceed under such vested rights as otherwise provided in the comprehensive plan and Land Development Code, and shall not be required by virtue of this Policy to seek rezoning to the Village Center standards if the property owner seeks no increase in either ~~densities OR intensity (as such term is defined in the Land Development Code)~~ beyond such vested rights, or ~~no increase~~ in density beyond 10% of vested dwelling units, as such terms "density" and "intensity" are defined in the Definitions section of this Plan.
4. The Land Development Code provides standards for four levels of development in the Village Center Area that will contribute to a walkable mixed-use environment in the Village Center Area:
 - a. Tier 1 provides a minimum network of connecting streets that will allow the general public and residents to move by car, bike, or on foot within and through development tracts.
 - b. Tier 2 accommodates residential neighborhoods with higher densities and a potential for a greater variety of housing types, as well as mixed-use neighborhoods with higher levels of non-residential uses, and, in each case, greater connectivity than Tier 1.
 - c. Tier 3 accommodates mixed-use neighborhoods with similar attributes as Tier 2 but with higher levels of non-residential uses as well.
 - d. Tier 4 allows an entire development tract to be planned as a compact community, as provided in Chapter 32 of the Land Development Code.
- E. Public Benefits: The Land Development Code provides minimum standards for each tier and describes public benefits that developers may offer to obtain specified density/intensity incentives in each tier. Public benefits shall be calibrated to the needs of each Tier and the particular proposed development plan, and must meet or exceed the goals and objectives of the comprehensive plan; meet or exceed the general land development code criteria for the Village Center; and create significant public benefit appropriate to the tier and the particular development plan.

Base and maximum residential densities will be set by the Village Council during the planned development rezoning process based on its determination of an application's compliance with this comprehensive plan and the specific standards and requirements for each tier. Increases in base residential densities may be allowed after consideration

of incentive offers as provided in the Land Development Code. Densities cannot exceed the top of the following ranges:

1. Tier 1: Base level is up to 6 dwelling units per acre of Tier 1-only land plus up to 3 additional dwelling units per acre of Tier 1-only land after consideration of accepted incentive offers, for a maximum of 9 dwelling units per acre of Tier 1-only land.
2. Tier 2: Base level is up to 10 dwelling units per acre of Tier 2 land plus up to 4 dwelling units per acre of Tier 2 land after consideration of accepted incentive offers, for a maximum of 14 dwelling units per acre of Tier 2 land.
3. Tier 3: Base level is up to 15 dwelling units per acre of Tier 3 land plus up to 5 dwelling units per acre of Tier 3 land after consideration of accepted incentive offers, for a maximum of 20 dwelling units per acre of Tier 3 land.
4. Tier 4: Base level is up to 21 dwelling units per acre of Tier 4 land plus up to 6 dwelling units per acre of Tier 4 land after consideration of accepted incentive offers, for a maximum of 27 dwelling units per acre of Tier 4 land.

Additional density above the base level up to the maximum density may be granted through the planned development process based on accepted incentive offers providing specific public benefits, such as enhanced interconnectivity, preservation of indigenous vegetation, innovative landscaping and street design, offsite improvements, dedication of onsite public spaces, public trails, gathering spaces, river trail easement, river buffer, civic building site, architectural excellence, enhanced parking design (under building parking), or other similar items as specified in more detail in the Land Development Code.

- F. Density Calculation: With respect to these base and maximum residential density calculations:
 1. For land in Tier 1-only, densities are calculated based on the definition of "Density" of the comprehensive plan, thus excluding non-residential land in Tier 1.
 2. For land in Tiers 2, 3, and 4, densities are calculated based on this definition except that non-residential land is included for these tiers only.

SPECIFIC USE CONSIDERATIONS

FLU-1.3

OBJECTIVE

In support of the unique character and quality of life in Estero, certain uses require enhanced consideration or review, or may be prohibited, as described in the policies below.



Image by Johnson Engineering, Inc.

POLICIES

FLU-1.3.1 To ensure that future commercial and mixed-use developments meet the community's planning priorities, all new commercial development which requires rezoning must be rezoned to a Commercial (CPD), Mixed Use (MPD), or Compact PD, as those districts may be amended from time to time. Commercial development shall not be established where traffic is required to travel through areas with significantly lower densities or intensities (e.g. commercial access through residential areas), except where adequate mitigation can be provided as determined acceptable through the public hearing process.

FLU-1.3.2 Except as set forth in Policy FLU-1.3.3 below, the following uses (as defined in the Land Development Code, as amended) are prohibited within Estero: "detrimental uses," nightclubs or bar and cocktail lounges unless within a Group III Restaurant or a hotel; and retail uses that require outdoor display in excess of one acre. Outdoor display in excess of one acre is only permitted in that part of the Transitional Mixed Use Future Land Use Category located west of 1-75, south of Corkscrew Road, and east of Corkscrew Woodlands Boulevard.

FLU-1.3.3 Nightclubs, bars, microbreweries and cocktail lounges, which are not within a Group III Restaurant or a hotel, may be considered in Transitional Mixed Use and Village Center designated areas through the public hearing process.

FLU-1.3.4 Warehouse, heavy commercial, and limited light industrial shall be allowed only in appropriate areas through rezoning to Planned Development.

FLU-1.3.5 Any proposal to convert a golf course to an alternate use shall be subject to review through the Planned Development rezoning process, and subject to standards in the Land Development Code that ensure preservation and protection of adjacent residential properties and open space views.

FLU-1.3.6 Existing bona fide agricultural uses shall be allowed to continue.

FLU-1.3.7 Location of churches and schools may be considered in all future land use categories except for Wetlands, Conservation and Public Parks and Recreation, provided

they shall not be established where traffic is required to travel through areas with significantly lower densities or intensities, except where adequate mitigation can be provided as determined acceptable through the public hearing process.

FLU-1.3.8 Consideration of public uses and public buildings, public utilities, public recreation uses (including franchised quasi-commercial uses in conjunction with a public use), and sites for compatible public facilities shall be allowed in all future land use categories when consistent with the goals, objectives and policies in this plan and applicable zoning and development regulations.

FLU-1.3.9 Any facility licensed under Florida Administrative Code Chapter 10A-5. (e.g. an adult congregate living facility) will be deemed a residential use and limited to locations and densities appropriate for residences, with the additional allowance to locate in the Urban Commercial category at a residential density standard of 1-6 du/acre, and subject to density conversions found in the LDC.

FLU-1.3.10 To maintain the Village's aesthetic standards, billboards are prohibited. The Village will implement an amortization program for existing billboards.

	BONUS DENSITY	FLU-1.4
OBJECTIVE	Density above the standard range can be considered in Transitional Mixed Use designated areas.	

POLICIES	FLU-1.4.1 Implementation of bonus density will be through the Land Development Code, with density amounts determined through the public hearing process.
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	COMMERCIAL SITE LOCATION STANDARDS	FLU-1.5
OBJECTIVE	<p>The siting of commercial developments shall be controlled by location standards. These standards are intended to avoid proliferation of commercial strip development by identifying appropriate locations for commercial uses to meet the needs of residents and visitors and to be compatible with existing residential neighborhoods or residential uses.</p>	
	 <p><i>Image by Johnson Engineering, Inc.</i></p>	

POLICIES

FLU-1.5.1 Commercial development levels are categorized as minor, neighborhood, community, or regional scale. The Village assigns intensities to these commercial levels according to the context and accessibility of the commercial development described below. See Map TR-1 for Functional Classifications of roadways.

The location standards specified in Subsections A-D will apply to the following commercial developments: shopping centers, free-standing retail or service establishments, restaurants, convenience food stores, automobile dealerships, gas stations, car washes, and other commercial development generating large volumes of traffic. These location standards will not apply to the following: banks and savings and loan establishments without drive-in facilities; hotels or motels; marinas; general, medical, or professional offices; industrial, warehouse, or wholesale development; clubs, as defined in the Land Development Code (commercial clubs excepted); and other similar development.

A. Minor Commercial

1. Provides for the sale of convenience goods and services and has a gross floor area less than 30,000 square feet.
2. Must be located (except where this plan provides specific exceptions) on or within 330 feet of the right of way of local and collector, local and arterial, or collector and collector roads; at the intersection of collector and arterial and arterial and arterial roads; or within a residential planned development provided it is located and designed primarily to meet the commercial needs of the residents of the development.
3. When developed as part of a Mixed Use planned development, Estero Planned Development or Compact planned development, and meeting the use limitations, modified setback standards, signage limitations, and landscaping provisions, retail uses may deviate from the locational requirements and maximum square footage limitations through compliance with design standards of this plan and approval by the Village Council.
4. Village Council may approve applications for minor commercial centers that do not comply with the location standards of subsection 2 above for such centers but which are consistent with design standards of this plan.

B. Neighborhood Commercial

1. Provides for the sale of specialty goods and convenience goods and personal services, such as food, drug, sundries, and hardware items and has an overall gross floor area range of 30,000 to 100,000 square feet.
2. Must be located at the intersection of an arterial and a collector or two arterials so that direct access is provided to both intersecting roads. Such direct access may be provided via an internal access road to either intersecting road.

- C. Community Commercial
 - 1. Provides for the sale of retail goods such as clothing, variety items, appliances, and furniture as well as goods that may be found in a neighborhood commercial development and has a gross floor area range of 100,000 to 400,000 square feet.
 - 2. Must be located at the intersection of two arterials so that direct access is provided to both intersecting roads. Such direct access may be provided by an internal access road to either intersecting road.
- D. Regional Commercial
 - 1. Provides some functions of community commercial, in addition to providing a full range and variety of shopping goods for comparative shopping (such as general merchandise, apparel, furniture, and home furnishings) and has a gross floor area range of 400,000 to 1 million square feet.
 - 2. Must be located in such a manner as to provide direct access to two arterial roads. Such direct access may be via an internal access road to one or more arterials.
- E. The Commercial Site location standards described in this policy do not apply to Regional Commercial development approved as part of the Coconut Point Development of Regional Impact.
- F. Commercial development "at the intersection" will extend no more than one-quarter mile from the centerline of the intersection and must include proper spacing of access points, with the following exception: in a commercial development approved under the planned development rezoning process, "at the intersection" may extend beyond one-quarter mile from the intersection, provided that:
 - 1. Direct access is provided to the development within one quarter mile of the intersection;
 - 2. An internal access road or frontage road provides access to the intersecting street prior to occupancy of the development; and
 - 3. All access points meet Land Development Code requirements for safety and spacing;
 - 4. The retail commercial use, including any outdoor sales, does not extend beyond one-half mile from the centerline of the intersection.

Standards specified in Subsections A-D for location and floor area will serve as guidelines during the rezoning process (allowing discretion by the Village Council in special cases in which retail uses are the only reasonable use of the parcel in light of its size, its proximity to arterials and collectors, and the nature of the existing and projected surrounding uses, including but not limited to environmental factors) but are strict requirements during the development order process. The descriptions specified in Subsections A-C will serve generally to indicate the types of development which are likely to fall within each

commercial development level. Proposed rezonings to commercial or mixed use zoning districts may be found consistent with the comprehensive plan by the Village Council even if the subject parcel does not comply with the applicable location standard and floor area; provided, however, that all such development orders must be consistent with the level of service requirements in this plan.

HISTORIC AND ARCHEOLOGICAL RESOURCES

FLU-1.6

OBJECTIVE

Where feasible, the Village will recognize and support the endurance of Estero's historic and archeological resources.



Image by Johnson Engineering, Inc.

POLICIES

FLU-1.6.1 The Village shall continue to use the Florida Department of State, Division of Historical Resources "Florida Master Site File" and Lee County's local register of historic places as the method of maintaining a record of local historically significant structures, sites, and housing.

FLU-1.6.2 The Village recognizes the area in the vicinity of the Koreshan Village (the 1883 Damkohler homestead) and the Boomer property on the west, the Broadway Avenue corridor on the north, Sandy Lane on the east, and Corkscrew Road on the south as an area appropriately designated as "Old Estero." Where feasible, through public/private recognition programs, the Village will encourage preservation of historic resources, sites and structures.

FLU-1.6.3 The Village will pursue appropriate cooperative ventures with private and public entities to encourage preservation of historic or archaeological sites in the Village. This includes fostering the presence of historical and cultural resources at the Estero Community Park or other suitable locations.

FLU-1.6.4 The Village will recognize the unique historical and cultural values of the Village of Estero by establishing and implementing development incentives and regulations within the Land Development Code that:

- A. Allow adaptive reuse of historic buildings, sites, and structures to provide property owners economic viability in the restoration and maintenance of these historic assets;
- B. Incorporate design features of Estero's historic structures into future architectural design, streetscape, and LDC standards in the vicinity of Old Estero;

- C. Explore methods to implement appropriate setback buffers along the Estero River to maintain the historic, scenic character of the watercourse; and
- D. Identify, protect and promote historic resources and facilities such as those located in Koreshan Park, the Old Estero area, and the Estero Community Park.

NONCONFORMITIES		FLU-1.7
OBJECTIVE	Provide for the reasonable and lawful protection of property owner investments where development approvals do not conform to comprehensive plan requirements or land development regulations enacted after the development approvals.	

POLICIES

FLU-1.7.1 VESTED RIGHTS. Nothing contained in this Comprehensive Plan shall modify or abridge the law of vested rights or estoppel under Florida Statutes or judicial precedent. Developments of Regional Impact maintain their statutory vested rights status pursuant to the provisions of Florida Statutes Chapter 380.06 and Section 163.3167(5). If the property owner is unclear as to the exact nature of the vested rights that are claimed, such property owner may submit an application to the Village of Estero for a determination of such vested rights. The Village Council will conduct a public hearing to determine the nature and extent of such vested rights, and shall apply judicially defined principles of equitable estoppel in making such determination. Each vested rights determination is based on the facts and law associated with that particular property and shall not be considered as a precedent that can be relied upon in any other determination.

FLU-1.7.2 BUILD BACK AFTER DESTRUCTION BY NATURAL FORCES. Structures which have been damaged by fire or other natural forces to the extent that the cost of their reconstruction or repair exceeds 50% of the replacement value of the structure may be reconstructed at (but not to exceed) the legally documented actual use, density, and intensity existing at the time of destruction, thereby allowing such structures to be rebuilt or replaced to the size, style, and type of their original construction, including their original square footage; provided, however, that the affected structure, as rebuilt or replaced, complies with all applicable federal and state regulations, local building and life safety regulations, and other local regulations which do not preclude reconstruction otherwise intended by this policy.

In order to reconstruct at the legally documented previous use, density, and intensity, a building permit must be applied for within three years after the date of destruction. The date of destruction must be legally documented. Such documentation may include a local, state, or federal declaration of disaster; a fire or police department report on the event; or any insurance claims filed as a result of the destruction. If a building permit is not applied for within three years of the destruction, the property will then become subject to current

regulations on use, density, and intensity. The Village shall maintain land development regulations to implement this policy.

FLU-1.7.3 LAND DEVELOPMENT REGULATIONS FOR NONCONFORMITIES. The Village will maintain land development regulations that provide for the appropriate regulation of nonconforming uses, structures and site improvements.

FLU-1.7.4 MINIMUM SINGLE-FAMILY RESIDENTIAL USE. Notwithstanding density provisions of this plan, any owner of land which land at the time of the adoption of the Comprehensive Plan is not in compliance with the standard density requirement for which the land is designated may construct a single-family residence on the land, provided that it meets certain standards for construction; lot or parcel recordation; minimum lot size; and access and drainage. These standards shall be further detailed in the land development regulations and shall be substantially similar to the standards for single-family residence administrative interpretations as set forward in Article XIII of the Lee Plan, dated August 2007.

ADMINISTRATIVE INTERPRETATIONS

FLU-1.8

OBJECTIVE

Provide an administrative process for interpretation of the Future Land Use Element to resolve matters limited to factual error in wetland designations and clarification of future land use map boundaries as to a specific parcel.

POLICIES

FLU-1.8.1 PROCESS. A landowner may apply to the Department of Community Development for an Administrative Interpretation delineated in this objective, using an application form provided by the Department and submitting information as may be requested in order to make an interpretation. Any appeal of the Administrative Interpretation shall be made to the Village Council, which shall hold a de novo quasi-judicial hearing on the appeal. Further standards and procedures for Administrative Interpretations and appeal shall be provided in the land development regulations.

FLU-1.8.2 WETLANDS ADMINISTRATIVE PROCESS. An administrative process shall be available to a landowner if a Wetlands boundary on the Future Land Use Map is incorrect due to a clear factual error, or if an exact boundary is established per jurisdictional determination or Florida Statute Section 373.019. The administrative process may be used to establish the precise boundary for the Wetlands designation on the Future Land Use Map.

FLU-1.8.3 FUTURE LAND USE MAP BOUNDARY CLARIFICATION. An Administrative Interpretation shall be available to a landowner to clarify the Future Land Use Map designation boundary of its specific parcel of property.

COMMUNITY DESIGN SUB-ELEMENT

DESIGN OF MIXED USE DEVELOPMENT

FLU-1.9

OBJECTIVE

The Village will encourage new development and redevelopment in mixed use patterns primarily in the Village Center and Transitional Mixed Use future land use categories, and on a very limited basis associated with the Village Neighborhood categories where appropriate and subject to compatibility standards. The objective is to promote Estero's quality of life and diverse local economy by fostering compatible mixed-use centers that can serve as central gathering places for Estero's residents, business people, and visitors.

POLICIES

FLU-1.9.1 MIXED USE DEVELOPMENT FORM. The Village encourages mixing of complementary residential and nonresidential uses on a single site to reduce vehicle miles travelled, support pedestrian, bicycle and transit opportunities and activate streets and public spaces with pedestrians for greater economic, recreational and social opportunities resulting in built environments and landscapes that provide a true sense of place.

- A. The Village Center and Transitional Mixed Use future land use categories are intended to accommodate mixed use development patterns, and the Neighborhood Village categories allow for a limited mix of use in suitable locations, subject to compatibility standards.
- B. Mixed use development shall be limited to uses that are complementary rather than conflicting, as those terms are specified in the definition of mixed use found [at the end of this Element in the Definitions section](#). The ultimate determination of allowable commercial uses will be through a public hearing process and will be based upon the needs of the Village, character of the surrounding area, the compatibility of the uses, and the characteristics of the transportation network.
 1. Carefully mixing complementary uses can reduce overall trip lengths, support pedestrian, bicycle and transit opportunities and create pedestrian friendly streetscapes. Mixed uses will be encouraged within individual buildings (e.g. residential above retail or office space) but may be located in separate buildings that can be easily reached using publicly accessible sidewalks and streets.

Mixed Use areas will provide for placemaking including, without limitation, public gathering places, civic uses, such as green spaces or community centers, and other placemaking public amenities described further below.

Mixed-use patterns will be integrated within an overall design framework to create a pedestrian friendly, human scale environment, through objective, measurable compatibility standards including size, scale, proportion, and materials detailed in

the land development regulations. Flexibility in design will allow for choice and variety in architectural style.

- C. Forms of mixed use development include horizontal mixed use, integrated horizontal mixed use, and vertical mixed use:



*Left: Typical single use building (Source: TownPlace Suites Marriott at Coconut Point)
Right: Typical vertical mixed use building (Source: Johnson Engineering, Inc.)*

1. Horizontal mixed use includes single use buildings on distinct parcels in one overall development project. This is the simplest form of mixed use in that basic proximity and pedestrian and bicycle connectivity between residences and nonresidential destinations affords the reduction of vehicle miles travelled. This form requires buffering between residential and nonresidential uses. Neighborhood commercial centers with direct pedestrian/bicycle infrastructure linking to residential areas is an example of horizontal mixed use.
2. Integrated horizontal mixed use includes single use buildings on distinct parcels in one overall development project. Integration of the project is achieved through placement of nonresidential uses within ¼-mile walking distance to residences, and with placemaking design elements so that separate uses are perceived as unified live, work, play, and shop destinations. Placemaking design elements are spaces and infrastructure meant to function for public access, use, and interaction. Context driven and accessible to the public through dedication of a public access easement, examples of placemaking design elements include indoor or outdoor spaces, walkable streets, environmental features, or public art.

Placemaking Examples

- a. Public art** (monuments, community engaged, professionally designed, fixed or temporary, etc.)



PGA Commons Palm Beach Gardens (Source: Google Earth)

- b. Urban green space** (passive open space, greenways, activated park, urban farms, etc.)



*Smale Riverfront Park
(Source: American Planning Association Image Library)*

- c. Plaza** (primarily solid surface for passive open space or activated with furnishings, furnishings can be fixed or flexible)



Coconut Point plaza space with fixed seating (Source: Johnson Engineering, Inc.)

d. Identifiable landmark (fountain, clock tower, etc.)



Bella Terra clocktower in Estero, Florida (Source: Google Earth)

e. Hidden off-street parking (mid-block parking structures with liner buildings)



Left: Mid-block parking in Coconut Point, Estero, Florida (Source: Google Earth)



Right: Parking structure (left) hidden by street-lining buildings (right) in Mizner Park, Boca Raton, Florida (Source: Google Earth)

f. Environmental feature (rain garden, signature trees, watercourse, roadside bioswale, etc.)



Left: Roadside bioswale intercepts stormwater from impervious surface (Source: National Association of City Transportation Officials (NACTO) text, "Urban Street Design Guide")



Right: Mercato development with signature tree in Naples, Florida (Source: Google Earth)

g. Paseo or via (pedestrian promenade free from cars)



*Left: Car-free zone from the Babcock Community Pattern Book, Kitson & Partners
Right: Fort Myers, Florida (Source: Johnson Engineering, Inc.)*

h. Traffic calming enhancements (roundabout or rotary, rumble strips, parklets, speed tables, etc.)

Placemaking requires bicycle and pedestrian infrastructure as appropriate per the Federal Highway Administration's list of proven safety countermeasures including (in coordination with local fire departments):

1. Safety Edge
2. Roundabouts
3. Corridor Access Management
4. Backplates with Retroreflective Borders
5. Longitudinal Rumble Strips and Stripes on 2-Lane Roads
6. Enhanced Delineation and Friction for Horizontal Curves
7. Medians and Pedestrian Crossing Islands in Urban and Suburban Areas
8. Pedestrian Hybrid Beacon

Implementation of a "Road Diet" or Roadway Reconfiguration to slow traffic and accommodate non motorized travel is also an example of a traffic calming enhancement.



*Left: Acacia roundabout in Clearwater, Florida (Source: Google Earth)
Right: Speed table in Naples, Florida (Source: Johnson Engineering, Inc. photo)*



Left: Mini circle in Coral Gables, Florida (Source: Google Earth)

Right: Brick paver cross walk through median in Naples, Florida (Source: Johnson Engineering, Inc. photo)

i. Increasing options of transportation (overlapping connections and multiple options for public transportation)



Left: Park & Ride in Davie, FL including parking spaces, bike racks and bus stops (Source: Google Earth)

Right: Public trolley in Sweetwater, Florida (Source: City of Sweetwater Transit)

j. Pedestrian enhancements (widened sidewalks exceeding 6 feet, pedestrian crossing islands, pedestrian hybrid beacon, paver surfaces for pedestrians, wayfinding signage, *etc.*)



Left: Pedestrian crosswalk in Hollywood, FL (Source: American Planning Association Image Library)

Right: Paver surface with shade for pedestrians (Source: Johnson Engineering, Inc. photo)

Recognizing that integrated design is more challenging, incentives for this form of development include higher allowable density and intensity, and relief from development standards which shall be implemented in the land development code, such as:

Buffering between uses is not required; cumulative parking requirements for integrated horizontal mixed-use projects could be reduced based on sharing of facilities for residential and nonresidential uses with alternating peak demands; stormwater facilities, utility and transportation infrastructure, and common area maintenance can also be shared.

3. Vertical mixed use includes residential and nonresidential uses in the same building. A development project with vertical mixed-use buildings as the primary building type (fewer than half of the buildings are single use buildings) constitutes a vertical mixed-use development project. Vertical mixed-use projects shall be integrated with placemaking design elements that are context driven and accessible to the public, so that the development project is perceived as unified live, work, play, and shop destination. Recognizing the increased complexity of permitting, constructing, financing, and managing vertical mixed-use projects, the Village shall implement incentives for this form of development including the highest allowable density and intensity, and relief from development standards which shall be implemented through the land development code, such as:

The allowable density per the Future Land Use category may be calculated using the gross acreage of the project; buffering between uses is not required; cumulative parking requirements for integrated horizontal mixed-use projects could be reduced based on sharing of facilities for residential and nonresidential uses with alternating peak demands; stormwater facilities, utility and transportation infrastructure, and common area maintenance can also be shared.

FLU-1.9.2 MIXED USE DEVELOPMENT STANDARDS. Development standards and criteria for mixed use development shall accomplish the following desired attributes:

- A. Provisions for outdoor livability, including interconnected pedestrian and bike facilities, public plazas, and seating.
- B. Well defined centers and edges with public or civic space creating an element around which other development is located. When necessary, development density and intensity will gradient from the center to the edge suitable to integrated surrounding land uses.
- C. Development plans will create focal points of signature buildings, civic spaces, natural amenities, and other prominent features through placement or street layout.

- D. Local climate and history will dictate the architectural and landscape design and natural methods of cooling and heating will be encouraged. Evaluate Green Building techniques as an alternative way to provide open space.
- E. Streets and roads will be fronted by design features including sidewalks which define and contribute to a pedestrian street character. Building design, placement, and entrances will be at a pedestrian scale and oriented towards streets or other public space such as parks or squares.
- F. The street system will be designed to provide safe access to, through (where feasible), and within each development in a way that equally serves automobile and non-automobile modes of transportation with the following considerations:
 - 1. Narrow traffic lanes and other traffic calming techniques to maintain safe multi-modal transportation.
 - 2. An interconnected street system with a pedestrian circulation system to connect the nonresidential uses with residential uses and areas, extended to adjacent sites at the least intrusive locations, with interconnections. The intent is to provide multiple vehicular, bicycle, and pedestrian linkages to adjacent local destinations, including residential neighborhoods, as an alternative to arterial and collector roads, except where such connections are precluded by physical layout of natural environmental features. Link pedestrian routes and bikeways with the street system or other public space such as parks or squares avoiding routes through parking lots and other locations out of the public realm.
 - 3. A system of alleyways for service vehicles and access to parking.
 - 4. Convenient access to transit facilities.
 - 5. Sidewalks shaded by rows of street trees.
 - 6. Automobile-oriented uses will have a limited number of driveways, and drive-in or drive-up windows will be located to avoid conflict with pedestrian and bicycle traffic. Auto-centric uses may not be allowed as incompatible with residential uses or will be strictly limited especially relative to the interface of pedestrian and bicycle traffic with drive throughs.
 - 7. Additional provisions to achieve the goals of development of walkable environments may be established in the Land Development Code.
- G. Large scale development design techniques should integrate the establishment into the surrounding community. Such design techniques will include:
 - 1. Creation of a series of smaller, well defined customer entrances to break up long facades and provide pedestrian scale and variety, that may be achieved through the use of liner buildings.
 - 2. Limited number and size of signs.

3. Landscaping and use of pocket parks and courtyards and other placemaking design elements adequate to soften large building masses.
- H. Crime Prevention Through Environmental Design (CPTED) guidelines will be incorporated to the maximum extent possible.
- I. Parking areas will be designed to minimize intrusiveness and impacts on the pedestrian character, through the following techniques:
 1. On-street parking with landscaping and design features, such as corner and mid-street bump outs, that afford traffic calming and produce a comfortable and safe pedestrian environment will be promoted.
 2. Parking lot locations will not separate pedestrian areas including sidewalks, squares, and plazas from the front of buildings containing the primary entrance.
 3. Parking lots will be screened from all streets, sidewalks, and open spaces, and will be designed to maintain or enhance the street edge.
 4. Parking lots will be designed with safe pedestrian connections to business entrances and public space to create a park-once environment.
 5. Reduction of paved parking areas will be evaluated wherever practicable through measures such as provision of shared parking and parking structures to serve multiple uses and alternative paving materials. Large expanses of pavement will be discouraged. Reduced ratios of required parking for non-residential uses for integrated horizontal mixed use and vertical mixed use projects will be evaluated in the land development regulations.
- J. Public space and landscaping will be provided that includes:
 1. Public areas will provide adequate urban landscaping that includes street trees, planted areas, and street furniture.
 2. Required surface and storm water management facilities will be designed integrally with civic spaces, and as a physical or visual amenity that provides usable open space or an aesthetic feature that resembles natural areas.
 3. Paved areas (including parking) will require overhead shading from tree canopy or building features based on factors such as scale of development and performance standards.
- K. Connectivity and Buffering: Mixed use developments will be well integrated both internally and externally.
 1. Automotive, pedestrian and/or bicycle connections to adjacent commercial development will be provided.
 2. Connections to existing residential neighborhoods will be encouraged and will not be precluded by the Mixed Use Development's design.

3. Buffering of uses internal to an integrated horizontal mixed use or vertical mixed use development are not required.
4. Buffering from adjacent developments, when deemed absolutely necessary, will not preclude future interconnectivity.

DESIGN OF COMMERCIAL USES

FLU-1.10

OBJECTIVE

Attractively designed and high quality commercial uses can be allowed in the Village Center and Transitional Mixed Use and Urban Commercial future land use categories, and in minor commercial and neighborhood centers of the Neighborhood Village Future Land Use category. The objective is to promote Estero's quality of life and diverse local economy by fostering the development of targeted economic areas, as a preference over the development of strip commercial centers, in order to provide a diverse employment and economic base while meeting the commercial, professional, and service needs of the people who live, work, and play within the community.



Image by Johnson Engineering, Inc.

POLICIES

FLU-1.10.1 COMMERCIAL DEVELOPMENT FORM. To permit orderly and well-planned commercial development at appropriate locations, all applications for commercial development will be reviewed and evaluated as to their compatibility with adjacent and nearby uses, including consideration of the following:

- A. Traffic and access impacts (rezoning and development orders);
- B. Architectural and landscape architectural design (rezoning and development orders);
- C. Site planning, interconnectivity, and public space (rezoning and development orders);
- D. Screening, landscaping and buffering (rezoning and development orders);
- E. Availability and adequacy of services and facilities (rezoning and development orders);
- F. Impact on adjacent land uses and surrounding neighborhoods (rezoning);
- G. Proximity to other similar centers (rezoning); and
- H. Environmental considerations (rezoning and development orders).

FLU-1.10.2 COMMERCIAL DEVELOPMENT STANDARDS. New commercial developments should be designed to arrange uses in an integrated and cohesive unit in order to address compatibility with the adjacent and nearby uses by adhering to the following standards:

- A. Provide visual harmony and screening;
- B. Reduce dependence on the automobile;
- C. Promote pedestrian movement within the development and connectivity to adjacent and nearby uses where such uses are compatible;
- D. Utilize joint parking, access and loading facilities;
- E. Avoid negative impacts on surrounding land uses and traffic circulation;
- F. Protect natural resources; and
- G. Provide necessary services and facilities where they are inadequate to serve the proposed use.
- H. Large scale nonresidential establishments will incorporate development design techniques to integrate the establishment into the surrounding community. Such design techniques will include:
 - 1. Creation of a series of smaller, well defined customer entrances to break up long facades and provide pedestrian scale and variety, that may be achieved through the use of liner buildings.
 - 2. Limited number and size of signs.
 - 3. Landscaping and use of pocket parks and courtyards adequate to soften large building masses.
- I. The Village shall evaluate establishing a review process for managing impacts of any new or expanding commercial development or use within a critical distance of an existing residential use or zoning district where residential use is allowed. Considerations of such a process may involve establishing limitations and controls associated with noise, light, security, aesthetic appearance, buffering, hours of operation and access to ensure compatibility of proposed commercial activity with nearby residences.

FLU-1.10.3 Encourage commercial developments within the Village of Estero to provide interconnect opportunities with adjacent commercial uses in order to minimize access points onto primary road corridors; and residential developments to provide interconnect opportunities with commercial areas, including, but not limited to, bike paths and pedestrian accessways. Connections to existing residential neighborhoods will be encouraged and will not be precluded by the commercial development's design.

DESIGN OF RESIDENTIAL USES

FLU-1.11

OBJECTIVE

To support Estero's quality of life, promote the community's unique character through the development of diverse, well-designed, and well-connected residential neighborhoods, and provide for the needs of multigenerational community by supporting a variety of housing types and neighborhood development forms, the Village will allow residential uses in the Village Neighborhood, Village Center, and Transitional Mixed Use future land use categories as outlined in Objective FLU-1.2.

POLICIES

FLU-1.11.1 RESIDENTIAL DEVELOPMENT FORM. New residential development should accommodate the mix and variety of housing types and configurations suitable to meet the needs and desires of current and future residents, with emphasis on safe means for residents to connect by walking or bicycling within neighborhoods and to the larger community.

FLU-1.11.2 RESIDENTIAL DEVELOPMENT STANDARDS. Support and enhance Estero's residential character by ensuring that development proposals address how they:

- A. Are compatible with adjacent uses, public facilities, and infrastructure systems;
- B. Impact surrounding environmental and natural resources, with specific controls to ensure preserved areas that are designated preserve through the development permitting process are not encroached upon and are only able to be altered or changed upon approval of amendments or modifications to applicable development orders and permits;
- C. Access, where applicable, nearby parks, public spaces, recreational facilities, greenways, blueways, and natural open spaces;
- D. Connect to adjacent residential developments, mixed-use centers, economic areas, public facilities, natural resources, and other community facilities; and
- E. Contribute to the overall design, landscaping, and aesthetics that make up the community's character as a harmonious place with beauty, spaciousness, and a diversity of high quality residential and commercial development that positively contributes to the quality of life of Estero's residents.

FLU-1.11.3 Encourage high quality and adaptive reuse for future sustainability by requiring three or more story multifamily buildings to have elevators, and encouraging multifamily buildings with parking on lower levels of the building.

FLU-1.11.4 Evaluate options to update the definitions in the land development code to ensure the maximum number of unrelated individuals who can occupy a dwelling is reasonably limited to protect the character and quiet enjoyment of single family and multifamily neighborhoods.

FLU-1.11.5 Maintain LDC landscape requirements for the maintenance and development of a well-designed and landscaped community while providing appropriate transitions between residential uses and surrounding areas. Such landscaping requirements may be greater between residential and commercial uses, while less stringent between differing uses within a mixed-use center.

CORRIDOR PLANNING

FLU-1.12

OBJECTIVE

Improve the appearance and function of the Village's corridors by coordinating transportation and land use considerations with emphasis on safety to encourage more multimodal use and human scale activity along the corridors.



Image by Johnson Engineering, Inc.

POLICIES

FLU-1.12.1 Along the Village's major corridors (Estero Parkway, U.S. 41, Corkscrew Road, and Three Oaks Parkway) and along the minor corridors (Broadway, Williams Road, Coconut Road, and Via Coconut Point), consider establishing corridor conscious development standards for the built environment that provide a pleasing and safe public realm for motorists, bicyclists, pedestrians and transit users. Such development standards should be based on planning for the various corridors to ensure these routes are functional and safe while identifying opportunities for enhanced conditions along the roadways to include, where appropriate: buildings oriented toward the street, bicycle racks, shaded pedestrian links and trails, plazas connecting from the right-of-way to buildings, transit stop accommodations, and buffering.

FLU-1.12.2 When considering corridor planning, priority should be placed on identifying opportunities for redevelopment and development along U.S. 41 north of Corkscrew Road in a way that captures the area's historic, natural and economic assets and enhances the area as a northern gateway into Estero.

FLU-1.12.3 Establish and promote Estero's unique character and identity by enhancing the community's boundaries through the use of gateway entry features such as ornamental landscape features, hardscape elements and Estero identification signs. Encourage the construction of gateways by working with Lee County and the Florida Department of Transportation and private property owners.

TRANSPORTATION

Connections

The transportation network is the framework for moving people and goods within the Village. The Village seeks an interconnected transportation network that successfully links residential, commercial and recreational areas safely, economically and efficiently. The core principles for the transportation network are intended to help accomplish the mobility of people and goods in sync with the Village's development pattern and future land use mapping.



Interconnections

A community that lacks interconnectivity contributes to congestion. Estero seeks to add interconnections between existing and newly developed areas to help shorten trips and make destinations easier to reach.

Linkages

Estero residents want to feel connected to their community's parks, recreation, and centers of activity. Pathways along roadways, greenways that are away from roadways, and blueways along the Estero River are desired to link residents to these community's assets. Opportunities to link to destinations beyond Estero along the CSX rail corridor and FPL corridor are also important.

Safety

Above all else, safety is the driving force behind the future planning, design, and maintenance of Estero's vehicular, bicycle and pedestrian infrastructure.

The majority of Estero's main corridors are under jurisdiction of Lee County or the State of Florida. The Village has ongoing and active coordination with those jurisdictions and the Metropolitan Planning Organization for the planning and management of these arterial and collector roadway corridors as well as regional pathways and transit. The goals, objectives and policies of the Transportation Element are intended help Estero grow as a community with multi-modal access and connections to and between Estero's neighborhoods, economic areas, civic uses, recreational facilities, natural and historic areas, and commercial and mixed-use centers.

Complete Streets

Complete street design strategies are responsive to the context and needs of the immediate area and surrounding community. Generally, complete streets provide safe, accessible, convenient routes for people in motorized vehicles, on bicycles and on foot.

Interconnectivity

New development and redevelopment projects can play a significant role in creating interconnections in locations where they are lacking. Interconnections between existing and new development areas and community assets like the parks, Estero River, and activity centers are valuable for reducing congestion and providing direct access between compatible uses.

Key Improvements

Due to their importance to traffic, safety and convenience, the need to make improvements along Corkscrew Road and at the proposed western entrance to Estero Community Park are highlighted.

Transportation Options

Blueways, bikeways, pedestrian paths and use of the CSX rail corridor and FPL corridor are important and viable alternatives to traditional roadway infrastructure. Estero is committed to identifying and expanding these options where and when feasible.

Roadscapes

Well-designed landscaping along roadways not only looks attractive but helps "complete" the street by providing shade and helping to cool and reduce the heat island effect. The Village will evaluate options for a Roadway Landscaping Master Plan to establish model standards to be applied in different contexts as appropriate to enhance the transportation system's integration with environmental, social, economic and aesthetic features of the Village.

Innovation and Technology

The Village anticipates the potential for accommodating autonomous vehicles in the future.

TRA-1

GOAL Provide a safe, economic, efficient, and interconnected transportation network that successfully links residential, commercial and recreational areas; and accommodates mobility in coordination with the Village's development pattern and Future Land Use Map.

COMPLETE STREETS

TRA-1.1

OBJECTIVE The Village shall improve multi-modal mobility and safety through implementation of complete street approaches, as appropriate.



Image by Johnson Engineering, Inc.

POLICIES **TRA-1.1.1** Provide for well designed, safe, and multi-modal transportation corridors by encouraging and establishing complete street design guidelines, as appropriate, for the major corridors (US 41, Corkscrew Road, Estero Parkway and Three Oaks Parkway) and minor corridors (Via Coconut/Sandy Lane, Broadway, Coconut Road, and Williams Road).

TRA-1.1.2 Complete street design guidelines shall include roadway and median landscape standards, access management guidelines, signage, street lighting, and sidewalks to ensure safe and effective pedestrian crossing within the context of a comprehensive pedestrian and bikeway system.

TRA-1.1.3 All public and private rights of way within future mixed-use centers and the Old Estero Area, as defined in the Land Development Code, are encouraged to be designed to include pedestrian ways, crosswalks and traffic calming measures including, where appropriate, on-street parking, raised crosswalks, narrow lane widths or other similar mechanisms.

TRA-1.1.4 Evaluate complete streets approaches to facilitate expansion of bikeways and walkways networks along roadway corridors that provide access to and from schools, parks, greenways and blueways. The Village shall consider improving the bicycle/pedestrian network with underpasses or overpasses where feasible.

INTERCONNECTIVITY

TRA-1.2

OBJECTIVE

Facilitate the development of an interconnected community that enables people to easily access Estero's neighborhoods, recreational areas, commercial and mixed-use centers as well as other areas within the county and region through an integrated transportation and mobility system.



Image by Johnson Engineering, Inc.

POLICIES

TRA-1.2.1 Encourage the use of landscaped paths and park areas to link neighborhoods, commercial and mixed use centers, and other open space and recreational facilities through an integrated system of bicycle, pedestrian, and roadways connections.

TRA-1.2.2 Establish land development code standards that ensure the development of a well connected transportation system that includes pedestrian pathways, bikeways, transit, and roadways. These standards should:

- A. Require, where feasible, interconnects with adjacent uses;
- B. To the extent feasible, minimize access points onto primary road corridors by providing multiple access to adjacent properties;
- C. Link neighborhoods, commercial and mixed-use centers, public facilities, and parks, with priority on corridors linking neighborhoods to each other and a corridor to link Estero Community Park, the Estero River, Koreshan State Park, and Estero Bay Preserve; and
- D. Enable multi-modal transportation access (pedestrian, bicycle, vehicular, and transit) within and between the different neighborhoods, economic and employment centers, civic uses, and public space, park, and recreational facilities within the Estero Community.

TRA-1.2.3 Expand opportunities for Estero's transportation network of pedestrian and bicycle pathways, sidewalks, trails, and other facilities by working with the State of Florida and other local and regional entities to:

- A. Consider multi-use pathways that feature shade trees, benches, bike racks, and other design elements to attract usage;
- B. Consider targeted funding sources including development contributions, private donations, public funding sources (e.g.: MSBU), or other mechanisms;
- C. Consider utilizing the FP&L right of way within Estero State Buffer Preserve for trail and recreational purposes by working with FP&L and Lee County;

- D. Support a pedestrian–bike trail within or along the rail right of way for public recreation by working with the existing rail corridor and private developers;
- E. Consider blueways in the Master Recreation & Open Space planning effort to determine appropriate and safe opportunities for motorized and non-motorized and recreational uses along the Estero River; and
- F. Coordinate with the MPO on developing the Estero Bicycle and Pedestrian Master Plan and recommend options to improve safety and quality of bicycle and pedestrian conditions and connections within the Village of Estero.

TRA-1.2.4 During the review of comprehensive plan amendments, rezonings, zoning amendments, and development order applications, applicants must provide an evaluation addressing potential for addition of new interconnections between communities, creation of pedestrian and vehicular interconnection, and addition of new connections to the surrounding road network.

TRA-1.2.5 Only take over maintenance responsibility from a private entity for roadways, trails, or multiuse paths on condition that those facilities will become open and accessible to the public.

TRA-1.2.6 Discourage cul-de-sacs in new development and redevelopment.

TRA-1.2.7 Coordinate with the City of Bonita Springs to evaluate the extension of Via Coconut Point Road to connect to Old 41.

CORKSCREW ROAD IMPROVEMENTS

TRA-1.3

OBJECTIVE

The Village shall seek to improve safety and traffic flow on Corkscrew Road.



Image from Google Earth

POLICIES

TRA-1.3.1 Work with the state and county to reduce truck traffic on Corkscrew road and encourage truck traffic to use Alico Road to avoid conflicts with the movement of residents and the elementary schoolchildren on Corkscrew Road.

TRA-1.3.2 Work with the state and county to improve traffic flow at the I-75 and Corkscrew Road interchange by implementing the "Interim Improvements" and collaborating on long term improvements.

TRA-1.3.3 Work with the state and county to improve traffic congestion on Corkscrew Road with a focus on the area from Three Oaks Parkway to Bella Terra Boulevard.

TRA-1.3.4 Discourage further development of increased density along [East Corkscrew Road](#) in the DR/GR area [as designated in the Lee County Future Land Use Map series](#).

TRAFFIC CONGESTION

TRA-1.4

OBJECTIVE

Reduce traffic congestion in the Village.



Image from Google Earth

POLICIES

TRA-1.4.1 Evaluate and prioritize recommendations from the "2017 Village of Estero Area Wide Traffic Study".

TRA-1.4.2 Periodically review road conditions and identify roadway segments, intersections, and safety improvements for potential inclusion in the Capital Improvements Plan.

SAFETY

TRA-1.5

OBJECTIVE

Consider public safety improvements when determining transportation infrastructure improvements.



Image by Johnson Engineering, Inc.

POLICIES

TRA-1.5.1 Use road conditions and vehicle, bicycle and pedestrian crashes historic data to identify improvements to be addressed in the Capital Improvements Plan.

TRA-1.5.2 Evaluate roundabouts as part of any new roadway construction or reconstruction project.

TRA-1.5.3 Ensure that critical evacuation roadway links receive high priority for maintenance and capital improvement expenditures. (see Policies CCM-1.1.4 and CCM-1.3.7)

TRA-1.5.4 During the review of rezonings or zoning amendments, and development order applications, applicants must provide an evaluation of vehicle, bicycle and pedestrian safety in the proposed design or plan.

TRA-1.5.5 The Village will comply with Lee County emergency evacuation policies and coordinate with the state and county on evacuation planning as needed.

TRA-1.5.6 New and expanded transportation facilities should be designed to protect existing residential neighborhoods.

MAJOR ROADWAY BEAUTIFICATION

TRA-1.6

OBJECTIVE

Enhance the aesthetics of roadways in the Village.



Image by Johnson Engineering, Inc.

POLICIES

TRA-1.6.1 Evaluate the need for a Roadway Landscaping Master Plan to establish model standards to be applied in different contexts to improve aesthetics of road rights of way and medians.

TRA-1.6.2 Coordinate with state and county transportation agencies with jurisdiction and with property owners to identify opportunities to ~~Encourage~~ appropriate landscaping within and along the rights of way of collectors, arterials and local roads to maintain and contribute to the attractiveness and high quality of the community.

TRA-1.6.3 Seek opportunities to provide a visually appealing and functional road landscape for the benefit of road users and for use as a linear recreational trail by the community.

TRA-1.6.4 Promote the design of transportation facilities to minimize impacts on the natural environment, enhance scenic views, and reflect the scale and character of surrounding natural and developmental features by using:

- A. Varying street widths and traffic calming measures to allow views of any scenic or historic resources;
- B. Appropriate paving materials, including intersection treatments;
- C. Appropriate styles of traffic control facilities; and
- D. Landscape materials that complement the character of the resources.

BLUEWAYS, BIKEWAYS AND PEDESTRIAN PATHS

TRA-1.7

OBJECTIVE

Maintain and enhance the Village's network of blueways, bikeways, and pedestrian paths.



Image by Johnson Engineering, Inc.

POLICIES

TRA-1.7.1 Evaluate blueways in the Recreation & Open Space Master planning effort to determine appropriate and safe opportunities for motorized and non-motorized and recreational uses along the Estero River.

TRA-1.7.2 Evaluate the feasibility of providing an Estero Bay water taxi service from the Estero River to destination in Estero Bay and Gulf of Mexico.

TRA-1.7.3 Support efforts of the Lee County Metropolitan Planning Organization to plan for hiking/biking/walking trails along the entire rail corridor, to add capability for commuter rail, light rail, or bus rapid transit service in Lee County and northern Collier County, and to maintain freight capability.

TRA-1.7.4 Seek Sun Trail funding to enhance the Village's network of bikeways and walkways, greenways and blueways.

TRA-1.7.5 Coordinate with the MPO on developing the Estero Bicycle and Pedestrian Master Plan and recommend options to improve safety and quality of bicycle and pedestrian conditions and connections within the Village of Estero.

TRA-1.7.6 Work with state, county and Florida Power & Light to allow the FPL easement which extends from Pelican Colony Blvd to Williams Road to be utilized as a bicycle and pedestrian path.

	LEVELS OF SERVICE	TRA-1.8
OBJECTIVE	Adopt and maintain transportation levels of service applicable within Estero.	

POLICIES

TRA-1.8.1 Minimum acceptable motorized vehicle Level of Service (LOS) Standards for roadways as established by the state of Florida and Lee County are recognized as shown below:

I-75	D
Arterials	E
Collectors	E

- A. ~~For general planning purposes roadway LOS may be evaluated using average annual daily traffic (AADT) counts~~For minimum acceptable levels of service determination, the peak season, peak hour, peak direction condition will be defined as the 100th highest volume hour of the year in the predominant traffic flow direction. The 100th highest hour approximates the typical peak hour during the peak season. Peak season, peak hour, peak direction conditions will be calculated using K-100 factors and "D" factors from the nearest, most appropriate county permanent traffic count station.
- B. ~~For purposes of concurrency determinations and issuing development orders the LOS shall be determined using P.M. peak hour traffic counts.~~
- C. Local Roads will be maintained in good order and repair.

TRA-1.8.2 Map TR-1 Roadway Classification Map is the functional classification map. The Village will review and update the Functional Classification Map when warranted by changes in the transportation system.

TRA-1.8.3 The Village will coordinate with the state of Florida and Lee County to encourage better traffic flow and functioning of arterial and collector roadways than the designated minimum levels of service on those roadways.

TRA-1.8.4 In order to maintain road network operating efficiency, minimum desirable standards for connections to the road network are established in the Land Development Code.

TRA-1.8.5 Develop an official Trafficways Map that will depict estimated ultimate right-of-way needs to provide for adequate level of service for the Village based on the development capacities of the Future Land Use Element.

TRA-1.8.6 Evaluate an alternative mobility level of service (LOS) to complement minimum acceptable annual average daily traffic (AADT) and peak hour/peak seasonal/peak direction roadway level of service (LOS) standards.

TRA-1.8.7 Support public transit and other modes of transportation to reduce traffic and provide services to transportation disadvantaged persons by coordinating with LeeTran and the MPO by:

- A. Maintaining availability of LeeTran schedules at Village Hall;
- B. Suggesting appropriate locations for new bus stops in the Village;
- C. Working closely with LeeTran to coordinate planning of the Village Center with the provision of public transit necessary to efficiently service patrons traveling to and within the area, including the possible establishment of a trolley system connecting the Village Center, parks and neighborhoods; and
- D. Continuing to support the provision of transportation services to serve the aging and disadvantaged population through the LeeTran Passport and other available programs.

TRA-1.8.8 Maintain an effective and fair system of impact fees to ensure that development creating additional impacts on arterials and collector roads pays an appropriate fair share of the costs to mitigate its (off-site) impacts.

ENERGY EFFICIENCY AND NATURAL RESOURCE PROTECTION

TRA-1.9

OBJECTIVE

The Village shall encourage reduction in transportation energy consumption and protection of natural resources.



Image by Johnson Engineering, Inc.

POLICIES

TRA-1.9.1 Promote design of subdivision street systems that incorporate multiple interior connections and routes that provide connections between major transportation arteries.

TRA-1.9.2 Promote design of traffic calming measures such as roundabouts.

TRA-1.9.3 Participate in the development of the MPO 2040 Long Range Transportation Plan (LRTP) and support travel demand management strategies aimed at reducing the number and length of car trips and increasing the efficiency of the transportation system.

TRA-1.9.4 Support new and emerging transportation innovations such as driverless or autonomous vehicles (AV) and their implications for passengers, services, and deliveries on roadways within the Village.

TRA-1.9.5 Seek to reduce energy consumption and greenhouse gases by promoting use of alternative fuels (e.g., electric vehicles, compressed natural gas).

TRA-1.9.6 Oppose new roads or expansion of existing facilities through wetland areas or the Density Reduction Groundwater Resource areas, as designated in the Lee County Future Land Use Map series, including the northward extension of CR951.

TRA-1.9.7 Protect, preserve and enhance native vegetation within and along the rights of way of collectors, arterials and local roads whenever possible.

RAIL CORRIDOR

TRA-1.10

OBJECTIVE

Evaluate the need for the CSX rail corridor as a strategic regional transportation corridor to protect the corridor for future transportation purposes (see Map TR-2) as recommended by the Lee County Metropolitan Planning Organization (MPO), and/or for the purposes of hiking, biking or walking trails.



Image by Johnson Engineering, Inc.

POLICIES

TRA-1.10.1 Support efforts of the Lee County Metropolitan Planning Organization to plan for hiking/biking/walking trails along the rail corridor and other corridors in the Village.

TRA-1.10.2 The Village Center future land use designation demonstrates the Village's commitment to transit-oriented development up to a half mile around future stations for commuter rail, light rail, or bus rapid transit. Transit-oriented development provides higher densities and intensities in a physical form that emphasizes walkability and connectivity and provides a broad range of uses, reducing reliance on vehicle trips and parking lots. Stations could be placed at Coconut Point and near Corkscrew Road. Model procedures for station area planning and implementation are provided in the Florida TOD Guidebook, published by Florida DOT in December 2012.

TRA-1.10.3 Encourage Florida DOT to purchase the real estate interests in the rail corridor from its current owner, CSX Transportation Inc. (which leases the corridor to Seminole Gulf Railway).

TRA-1.10.4 Oppose attempts to abandon the rail corridor, with the exception of abandonment, to further the Village's initiative for pedestrian and biking trails. Support use of federal rails-to-trails authority to railbank the corridor, if abandonment ever succeeds, in order to preserve the corridor for possible future rail service or recreational needs.

TRA-1.10.5 Cooperate with Lee and Collier Counties, the City of Bonita Springs, and the City of Fort Myers in evaluating and potentially operating public rail transportation through their jurisdictions and creating new redevelopment opportunities near potential stations.

TRA-1.10.6 Cooperate with all counties and cities along the rail corridor to pursue common goals for trails.

HOUSING

Homes

Estero's unique character is embedded in the successful, consistent quality of its residential neighborhoods. Core housing principles are aimed at providing an adequate variety of housing which meets the needs of Estero's existing and future residents consistent with the character of the Village.



Rendering of Genova from estero-fl.gov

Value & Character

Preserving and building the value of established residential housing stock is a goal of most communities, but it is at the heart of the vision for the Estero community. A community of neighborhoods, Estero's architectural and natural features define its character. This character is to be maintained and furthered through application of codes and standards for the maintenance, development, and redevelopment of housing.

Historic Preservation

The Village supports recognition, restoration, maintenance and adaptive reuse of historic homes to ensure these connections to the community's past remain viable and useful for present and future generations.

Variety

Estero currently has a wide array of housing options. The Village seeks to maintain a variety of housing options to serve all needs, including for aging and special needs populations.

Sustainability

Housing developed in Estero should be constructed to stand the test of time, maximize resource efficiency, and serve the changing needs of current and future generations.

The goals, objectives and policies of the Housing Element are intended to ensure the existing and future housing stock of Estero is of high value and meets the needs of the community.

Neighborhood Integrity and Compatibility

Protect the integrity of residential areas by ensuring that new developments are compatible and consistent in form and character.

Recognition of Historic Housing

Encourage the preservation, restoration or rehabilitation for adaptive reuse of historically significant housing. Consider incorporating design characteristics of these historic structures into future architectural design, streetscape, and community-wide standards.

Aging and Special Needs Populations

Allow for private and non-profit sectors to provide dwelling units of various types, sizes and costs to meet the housing needs of residents, businesses, and elderly and special needs populations while maintaining the unique character of the Village.

Affordable Housing

Maintain affordable housing stock and allow for the private sector to provide housing for the workforce and housing that is affordable. Affordable housing options are possible through the rehabilitation of the Village's older housing stock.

Sustainable Strategies

Maximize resource efficiency in the built environment by supporting sustainable construction methods and use of renewable energy.

Maintenance of Housing Stock

The Village will ensure proper maintenance of the housing stock through code enforcement guidelines for the rehabilitation or reconstruction of any housing that becomes substandard.

HOU-1

GOAL

Provide an adequate mix of safe and sustainable housing which meets the needs of Estero's existing and future residents.

RESIDENTIAL NEIGHBORHOODS

HOU-1.1

OBJECTIVE

Preserve the value and character of existing neighborhoods.



Image by Johnson Engineering, Inc.

POLICIES

HOU-1.1.1 Recognize the value of strong and stable neighborhoods by encouraging neighborhood identity and a sense of community in the Village.

HOU-1.1.2 Promote public/private programs and activities that strengthen, stabilize, improve and enhance neighborhoods.

HOU-1.1.3 New development must be compatible with existing residential uses.

HOU-1.1.4 Protect residential neighborhoods from encroachment and detrimental impacts from other more intensive land uses and continue to enforce existing regulations that provide buffering from more intensive uses.

HOU-1.1.5 The Village shall promote programs designed to enhance neighborhood safety in order to help prevent possible housing deterioration by crime. Facilitate public participation in the land development approval process by having the owner or agent applying for Planned Developments, Rezoning, Variances, Special Exceptions, Plan Amendments, and other amendments and Development Orders, within the Village, participate in a public information meeting pursuant to adopted Village regulations and policies.

AGING AND SPECIAL NEEDS POPULATIONS

HOU-1.2

OBJECTIVE

Ensure the availability of adequate housing for the elderly and special needs populations.

POLICIES

HOU-1.2.1 In order to serve persons with special needs and disabilities, the Village will allow community residential facilities according to the definition and siting provisions of Florida Statute 419.001.

HOU-1.2.2 Adult living and continuing care facilities should be located in appropriate areas and should be convenient to services.

HOU-1.2.3 Cooperate with ~~appropriate agencies~~ public and/or private housing providers in order to provide adequate sites for group homes, foster care facilities and similar facilities to meet the requirements of persons with special needs.

HOU-1.2.4 Promote the design of home features that will help elderly and disabled persons to live independently.

HISTORIC PRESERVATION

HOU-1.3

OBJECTIVE

Encourage the preservation, restoration or rehabilitation for adaptive reuse of historically significant housing, as appropriate.



Image by Johnson Engineering, Inc.

POLICIES

HOU-1.3.1 Continue to encourage preservation of buildings, sites, districts and other resources of historic significance in Estero and maintain Land Development Code standards for historic preservation.

HOU-1.3.2 Require that modifications to any housing within the Village designated as historically significant by a recognized county or state agency comply with the U.S. Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings.

HOU-1.3.3 Recognize the unique historical and cultural values of the Village of Estero by establishing and implementing development incentives and regulations within the Land Development Code that:

- A. Encourage the development of the Old Estero area in a manner that maintains its historic character;
- B. Incorporate design features of Estero's historic structures into future architectural design, streetscape, and community-wide LDC standards; and
- C. Identify, protect, and promote historic resources and facilities such as those related to Koreshan Park, Old Estero area, and the Estero Community Park.

HOU-1.3.4 Consider creating an inventory of buildings which are 50 years or older, including their unique characteristics, to provide criteria or standards for any restoration/conservation action.

SUSTAINABLE HOUSING

HOU-1.4

OBJECTIVE

Encourage sustainable housing design and construction practices.

POLICIES

HOU-1.4.1 Encourage adaptive reuse of buildings where appropriate.

HOU-1.4.2 Encourage installation of solar panels and LEED (Leadership in Energy and Environmental Design) construction methods.

HOU-1.4.3 Encourage energy efficiency in new and existing housing to reduce the production of greenhouse gas emissions.

HOU-1.4.4 Promote materials conservation, including materials reuse, construction and demolition materials recycling, and use of recycled content materials.

HOUSING AVAILABILITY AND VARIETY

HOU-1.5

OBJECTIVE

Allow for a mix of dwelling units of various types, sizes, and costs, to meet the housing needs of the community, businesses and residents.



Rendering of Genova from estero-fl.gov

POLICIES

HOU-1.5.1 The Village's Future Land Use Element and Future Land Use Map will include a variety of residential land use categories to accommodate varying housing densities and housing types to provide opportunities for affordable workforce housing; housing for low-income, very low-income, and moderate-income households; mobile homes and manufactured homes; and community residential facilities.

HOU-1.5.2 Encourage a mix of housing types in areas designated for mixed use.

HOU-1.5.3 Encourage the rehabilitation of older housing stock into affordable housing.

HOU-1.5.4 The Village shall not prohibit mobile and manufactured homes as affordable housing in appropriately zoned areas.

HOU-1.5.5 The Land Development Code will continue to designate zoning categories for mobile home parks.

HOU-1.5.6 As part of the ongoing efforts to provide additional affordable housing opportunities, the Village shall encourage housing alternatives to be provided for Village residents displaced by the closure of mobile home parks.

HOU-1.5.7 The Village will continue to assist community based organizations and community housing development organizations to address housing needs in the Village and evaluate ways to streamline the permit process.

HOU-1.5.8 Workforce and affordable housing shall be consistent with the use and density provisions of the Comprehensive Plan and Land Development Code and located in areas that:

- Avoid concentrations of very-low and low-income households;
- Are provided full urban services and facilities;
- Are environmentally sensitive; and
- Would create a livable and supportive environment.

SUBSTANDARD HOUSING

HOU-1.6

OBJECTIVE

Ensure that housing remains safe and sanitary through code compliance and enforcement.

POLICIES

HOU-1.6.1 Require unsafe or condemned residential or commercial structures to be secured, repaired, demolished, or otherwise removed as a threat to the public health, safety, and welfare.

HOU-1.6.2 Utilize available code enforcement procedures to ensure that existing structures meet code standards.

HOU-1.6.3 Enforce the Florida Building Code in all new development and redevelopment to avoid substandard housing conditions.

HOU-1.6.4 Facilitate the redevelopment of older properties to come into compliance with adopted comprehensive plan policies, Land Development Code and building standards.

HOU-1.6.5 Evaluate implementation of a property maintenance code.

INFRASTRUCTURE

Services

To ensure the public health, safety, and welfare of the community, the infrastructure element calls for coordination with service providers to deliver high quality water, sewer, drainage, and solid waste services and facilities.



Service Levels

The Village adopts level of service standards for the basic essential services of water, sewer, drainage, and solid waste. These standards are consistent with the standards adopted by the providers of those services to ensure infrastructure is in place and adequate to serve existing and future populations.

Drinking Water

Proper planning to preserve and protect the drinking water supply is an important responsibility to ensure the health and welfare of current and future generations. Water supply planning is coordinated with the utility service providers and the South Florida Water Management District. Ground water protection is coordinated with local and state agencies.

Surface Water

Natural water bodies, flowways and manmade water management systems are abundant in Estero. These systems are essential for proper flood protection and water quality. The Village seeks to protect the natural features and ensure maintenance of the manmade features.

Green Infrastructure

Natural areas, drainage systems, and open spaces are vital to the sustainability of the built environment. The Village supports innovative techniques in infrastructure design that serve the needs of the community with emphasis on the value and importance of natural or green infrastructure.

Essential services of water, sewer, drainage, and solid waste systems are provided to Village residents through the operations and regulation of other agencies. The goals, objectives and policies of the Infrastructure Element are intended to ensure essential services are provided in a coordinated manner that ensures safety, sustainability and cost-effectiveness.

Water Services and Supply

Coordination with Lee County Utilities and Bonita Springs Utilities will ensure the provision of acceptable levels of potable water service. The Village also coordinates with the South Florida Water Management District in adopting a Water Supply Facilities Work Plan, and coordinates with County and state agencies on implementing standards to protect groundwater.

Sewer Services and Conversion from Septic

Much of the Village is served with sewer systems of Lee County Utilities and Bonita Springs Utilities. The Village will coordinate to ensure provision of acceptable levels of sewer services. Where sewer systems are not in place, it is priority to abate and cease use of septic tanks and wastewater treatment package plants where and when central sewer is available in order to preserve public health and preserve and enhance environmental quality.

Solid Waste

The existing solid waste facilities serving Estero are operated by Lee County. Estero adopts the minimum acceptable level-of-service standard for availability of solid waste disposal facilities.

Surface Water Management

The Village will adopt a Storm Water Management Master Plan. This will address water quality issues and the protection of present and future uses of real property from stormwater flooding.

Green Infrastructure

The following techniques will be evaluated as options for new capital projects, and will be encouraged when and where possible: green roofs, permeable pavement, bioswales, rainwater harvesting, green streets, stormwater parks, and conservation areas.

INF-1

GOAL

To ensure the public health, welfare, and safety by providing and coordinating with providers to deliver high-quality water, sewer, drainage, and solid waste services and facilities throughout the Village of Estero, and to ensure that the costs of providing services and facilities are borne by those who benefit from them.

SURFACE WATER MANAGEMENT

INF-1.1

OBJECTIVE

Reduce the hazards to life, health, and property created by flooding due to rainfall in a manner consistent with the community's criteria for the preservation of environmental values and the conservation of natural resources.



Image by Johnson Engineering, Inc.

POLICIES

INF-1.1.1 The following surface water management standards are adopted as minimum acceptable levels of service:

- A. The existing surface water management system in the Village will be sufficient to prevent the flooding of designated evacuation routes (U.S. 41, Corkscrew Road, Three Oaks Parkway, Interstate 75, and Ben Hill Griffin Parkway) from the 25-year, 3-day storm event (rainfall) for more than 24 hours.

~~All new private and public and private infrastructure shall be constructed with floor slabs constructed a minimum of one (1) foot above the 100-year, 3-day storm event flood plain level and shall be safe from flooding from a 100-year, 3-day storm event (rainfall).~~

- B. Regulation of Private and Public Development

All new private and public developments shall be constructed with floor slabs constructed a minimum of one (1) foot above the 100-year, 3-day storm event flood plain level and shall be safe from flooding from a 100-year, 3-day storm event (rainfall). In addition, in such new public and private developments, road elevations shall be constructed at a minimum of the 25-year, 3 day storm event flood plain level, and parking areas shall be constructed at a minimum of the 10-year, 1 day flood plain level.

Surface water management systems in new private and public developments (excluding widening of existing roads) must be designed to SFWMD standards (to detain or retain excess stormwater to match the predevelopment discharge rate for the 25-year, 3-day storm event [rainfall]).

Stormwater discharges from development must meet relevant water quality and surface water management standards as set forth in state rules including but not limited to requirements listed in Numeric Nutrient Criteria and Total Maximum Daily Loads and Basin Management Action Plans.

New developments must be designed to avoid increased flooding of surrounding areas ~~and be consistent with the adopted recommendations of the Village Stormwater Master Plan~~. Development must be designed to minimize increases of discharge to public water management infrastructure (or to evapotranspiration) that exceed historic rates, to approximate the natural surface water systems in terms of rate, hydroperiod, basin and quality, and to eliminate the disruption of wetlands and flowways, whose preservation is deemed in the public interest.

- C. Design trunk conveyance crossings of arterial roads to be free of flooding from 25-year, 3-day storm events at a minimum.
- D. Other standards may be added by amendment to this comprehensive plan for development impacting water bodies that have water quality impairments identified by Florida Department of Environmental Protection, including but not limited to the following water bodies: 3258C4 - Mullock Creek (Marine Segment); 3258C - Mullock Creek; 3258D1 - Estero River (Marine Segment); 3258D2 - Estero River; 3258H2 - Spring Creek (Marine Segment).

INF-1.1.2 Prepare a Stormwater Master Plan that is geographically based on watersheds using best available data related to rainfall, soils, topography, drainage patterns and water quality.

The Stormwater Master Plan will evaluate future conditions (including build-out within the watershed) and long-duration storms, identify existing wetlands and natural areas, and promote the protection of natural channels and conveyances. The Stormwater Master Plan will also provide recommendations on mitigation activities for the enhancement of existing stormwater management facilities and/or natural conveyances and recommend other programmatic and regulatory action items to be implemented within the land development code with the intent to reduce increased flooding from future conditions within the watershed and enhance water quality.

INF-1.1.3 The Village shall implement its Stormwater Master Plan through the land development code and through facilities and improvements intended to protect present and future uses of real property from stormwater flooding and to address water quality issues.

~~INF-1.1.4 Protect and restore principal flow-way systems, if feasible, to assure the continued environmental function, value, and use of natural surface water flow ways and associated wetland systems~~The Village shall endeavor to protect, secure and restore, where feasible, natural flowway systems including, without limitation, the Main, South and North Branches of the Estero River and the Halfway Creek flowway, through regulations or incentives to assure their continued environmental function.

INF-1.1.5 The Village shall maintain requirements in its land development code that proper stormwater management systems be installed when land is being redeveloped. Appropriate exemptions will be provided to this requirement for individual residential structures and for historic designated properties. However, this policy will not be interpreted so as to waive any concurrency level-of-service standards

INF-1.1.6 Water management projects will be evaluated and ranked according to the priorities adopted into this plan and the Village's adopted Stormwater Master Plan. Major emphasis will be given to improving existing drainage facilities and to enhancing or restoring environmental quality.

INF-1.1.7 Provide sufficient performance and/or design standards for development protective of the function of natural drainage systems, including the following provisions to be addressed in the land development code:

- A. Provide sufficient performance and design standards to require post-development runoff to approximate the total characteristics of the natural flow prior to development.
- B. Floodplains must be managed to minimize the potential loss of life and damage to property by flooding.
- C. Floodways should be kept as unobstructed as possible.
- D. Natural flow patterns will be restored by public investment where such action is of significant public or environmental benefit, and feasible.
- E. To maintain flowways, the Village may coordinate with landowners at time of new development or redevelopment to establish flowage easements while allowing concentration or clustering of development on the remainder of the property.
- F. The Village will maintain regulations which provide for the management and protection of floodplains, consistent with state and federal regulations.
- G. Through the land development code, the Village will require developments to have and maintain an adequate surface water management system that is consistent with the criteria adopted per recommendations of the Stormwater Master Plan, provide for acceptable programs for operation and maintenance, and post-development runoff conditions which reflect the natural surface water flow in terms of rate, direction, quality, hydroperiod, and drainage basin.

- H. Channelization of natural streams and rivers is prohibited; channelization of other natural watercourses is discouraged.
- I. Through the land development code, the Village will adopt appropriate regulations to protect the natural functions of riparian systems from incompatible development practices along their banks.

GROUNDWATER RECHARGE

INF-1.2

OBJECTIVE

Protect groundwater supplies from those activities having the potential for depleting or degrading those supplies.

POLICIES

INF-1.2.1 The Village shall implement Lee County's wellfield protection ordinance through the land development code to protect the quality of water flowing into potable water wellfields.

INF-1.2.2 The Village shall coordinate with the Lee County staff hydrogeologist for review and comment on all development applications near public utility potable water wellfields, with particular attention to proposed land uses within a 10-year travel time from the wellheads.

INF-1.2.3 The Village shall base all future development and use of groundwater resources on determinations of the safe yield of the aquifer system(s) in order not to impair the native groundwater quality or create other environmental damage. Criteria for safe-yield determinations will be determined by the SFWMD, the agency charged with permitting these activities.

INF-1.2.4 For maximum protection of groundwater resources, coordinate with applicable utility providers to identify future wellfields and/or relocation site(s) for existing wellfields well in advance of need. Coordinate with SFWMD, water suppliers, and Florida Department of Environmental Protection to avoid duplication and to assist in data collection.

INF-1.2.5 Coordinate with Lee County and utility providers to identify water needs consistent with projections of human population and the needs of natural systems in order to determine the future demands for groundwater.

INF-1.2.6 Continue to encourage the developer driven expansion of infrastructure to provide reuse water service when sufficient supply is available.

INF-1.2.7 When and where available, reuse water should be the first option for meeting irrigation needs of a development. Where reuse water is not available, surface water or low quality groundwater should be used for irrigation. All other potential water sources must

be eliminated prior to selecting potable water as the sole source for meeting the irrigation needs of a development.

WATER SUPPLY PLANNING & COORDINATION

INF-1.3

OBJECTIVE

Coordinate with the South Florida Water Management District to ensure that the Village Comprehensive Plan remains consistent with the District's regional water supply plans.

POLICIES

INF-1.3.1 No building permit will be issued unless potable water supply will be available to meet current and projected growth demands, or surety is given that it will be available prior to occupancy.

INF-1.3.2 The Village shall develop a Water Supply Facilities Work Plan within 18 months after the South Florida Water Management District approves an update to the regional water supply plan.

INF-1.3.3 The Village shall continue to evaluate the latest water supply plans issued by the South Florida Water Management District to ensure consistency in the Village Comprehensive Plan and the Village's Water Supply Facilities Work Plan.

INF-1.3.4 The Village shall coordinate with other government agencies and private suppliers of potable water during the water supply planning process to include the review of land use changes, addressing population projections, and acceptable level of service standards.

SANITARY SEWER

INF-1.4

OBJECTIVE

Coordinate with utility providers to ensure the provision of acceptable levels of sanitary sewer service.

POLICIES

INF-1.4.1 The minimum acceptable level of service standard for sanitary sewer connections to Lee County Utilities is established for the basic facility capacity, which is the principal productive capital of a water or sewer system, i.e., a wellfield and water treatment plant, as distinguished from the distribution system. The minimum acceptable level of service for Lee County Utilities sanitary sewer will be:

Available basic facility capacity to treat and dispose of a volume of wastewater equal to 200 gallons per day per equivalent residential connection (ERC), except that facilities serving only multifamily or mobile home residential structures shall have a capacity of 160 gallons per day and facilities serving only recreational vehicles or travel trailer residential structures must have a capacity of 80 gallons per day.

INF-1.4.2 The minimum acceptable level-of-service standards for sanitary sewer connections to Bonita Springs Utilities will be:

Available capacity to treat and dispose of a volume of wastewater equal to 200 gallons per day per equivalent residential connection (ERC), except that facilities serving only mobile home residential structures shall have a capacity of 150 gallons per day and facilities serving only travel trailer residential structures shall have a capacity of 120 gallons per day.

INF-1.4.3 Encourage all private utilities to set a minimum acceptable level of service to be adopted herein for use in the concurrency management system within their respective franchised or certificated areas.

INF-1.4.4 All utility providers are encouraged to construct and install sufficient treatment facilities and collection systems that will meet or exceed the minimum acceptable service standards and with the capacity to service the demand so generated and will meet or exceed the minimum requirements of the Department of Environmental Protection, the Department of Health and Rehabilitative Services, U.S. Environmental Protection Agency, or any local ordinances which exceeds the foregoing. All utility providers must advise the planning and utility engineering staff of the Village regarding system expansions or modification and must regularly provide summaries to the Village on compliance with standards.

INF-1.4.5 No development order for new development, or change in use or intensity in an approved development order, will be issued unless sanitary sewer service is available at the minimum acceptable level of service within 1/4 mile of the development, or surety is given that it will be installed prior to occupancy.

INF-1.4.6 No permit will be issued allowing any utility to use a public right-of-way or to cut a pavement in a public right-of-way to extend service outside of its certificated or franchised area or to extend service into an area allocated to another utility, unless the other utility concurs in writing. This will be enforced along municipal and state rights-of-way by interlocal agreement and memoranda of agreement as required.

INF-1.4.7 The costs of new or augmented sanitary sewer infrastructure that is developed by a utility provider will be borne by those who benefit from the improved supply.

INF-1.4.8 New development will pay through appropriate financial mechanisms its fair share of the costs of providing standard sanitary sewer for that development.

INF-1.4.9 It is hereby declared that in the interests of preserving public health and of preserving and enhancing environmental quality, it is in the public interest to abate and cease use of septic tanks and wastewater treatment package plants where and when central sewer is available. The Village will identify the existing inventory of septic tanks in the Village, evaluate water quality issues associated with them, and coordinate with property owners, local utility providers and Florida Department of Environmental Protection to develop a program, schedule and funding to convert from septic tank use to central sewer facilities. Abatement of the use of private potable water wells and conversion to central water facilities should be considered in conjunction with this program.

INF-1.4.10 With the cooperation of the respective utility firms or agencies, the Village will coordinate as needed on programs for the abatement of septic tanks and package plants or potable wells in areas in which central sewer or water is presently available and in areas encompassed by assessment districts established for upgrading central sewer or water availability.

	POTABLE WATER	INF-1.5
OBJECTIVE	The Village of Estero shall coordinate with utility providers to ensure the provision of acceptable levels of potable water service.	

POLICIES	<p>INF-1.5.1 The minimum acceptable level of service standard for potable water connections to Lee County Utilities will be:</p> <p>An available supply and treatment capacity of 250 gallons per day per equivalent residential connection (ERC), except that facilities serving only multi-family or mobile home residential structures must have a capacity of 200 gallons per day and facilities serving only recreational vehicle or travel trailer residential structures must have a capacity of 100 gallons per day.</p> <p>INF-1.5.2 The minimum acceptable level of service standard for potable water connections to Bonita Springs Utilities will be:</p> <p>An available supply and treatment capacity of 235 gallons per day per equivalent residential connection (ERC) and delivery of potable water at a minimum pressure of 20 pounds per square inch (psi) at the meter anywhere in the system.</p>	
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INF-1.5.3 Utility providers are encouraged to construct and install sufficient treatment facilities and distribution systems that will meet or exceed the minimum acceptable service standards and with the capacity to deliver water at a pressure of 40 pounds per square inch (wp PSI static) at the meter anywhere on the individual system (excluding fire flow conditions). All utility providers must advise planning and engineering staff of the Village regarding system expansions or modifications and must regularly provide summaries to the Village on compliance with water treatment and quality standards.

INF-1.5.4 No development order for new development will be issued unless potable water service, at the minimum acceptable level of service, is available at the property line, or surety is given that it will be installed prior to occupancy.

INF-1.5.5 No permit will be issued allowing any utility to use a public right-of-way or to cut a pavement in a public right-of-way to extend service outside of its certificated or franchised area or to extend service into an area allocated to another utility, unless the other utility concurs in writing. This will be enforced along municipal and state rights-of-way by interlocal agreement and memoranda of agreement as required.

INF-1.5.6 The costs of new or augmented potable water infrastructure that is developed by a utility provider will be borne by those who benefit from the improved supply.

INF-1.5.7 New development will pay through appropriate financial mechanisms its fair share of the costs of providing standard potable water for that development.

GREEN INFRASTRUCTURE

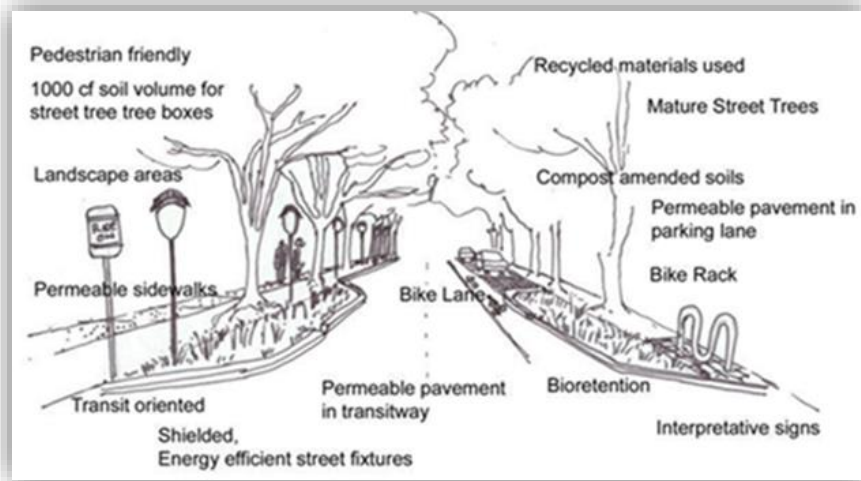
INF-1.6

OBJECTIVE

The Village seeks to foster and maintain and ensure compliance with requirements associated with its green infrastructure, which is comprised of natural areas, low impact drainage systems, open spaces, and green energy resources, in support of public health, safety, welfare and resource sustainability.

POLICIES

INF-1.6.1 Encourage when possible the following techniques: green roofs, permeable pavement, bioswales, rainwater harvesting, green streets, stormwater parks, and conservation areas.



Anatomy of a green street from www.epa.gov

INF-1.6.2 When the Village evaluates a proposed capital improvement to the stormwater system, consideration will be given to green infrastructure alternatives and retrofits according to: cost estimates, life cycle costs including capital and operation and maintenance, potential disproportionate burdens on portions of the community, projected pollutant reductions, benefits of receiving waters and other environmental and public health benefits associated with each option.

SOLID WASTE

INF-1.7

OBJECTIVE

Support the health, safety, and general welfare of Estero's residents by protecting the quality of the environment through the proper management and disposal of solid waste.

POLICIES

INF-1.7.1 The minimum acceptable level-of-service standard for availability of solid waste disposal facilities is 7 pounds per capita per day.

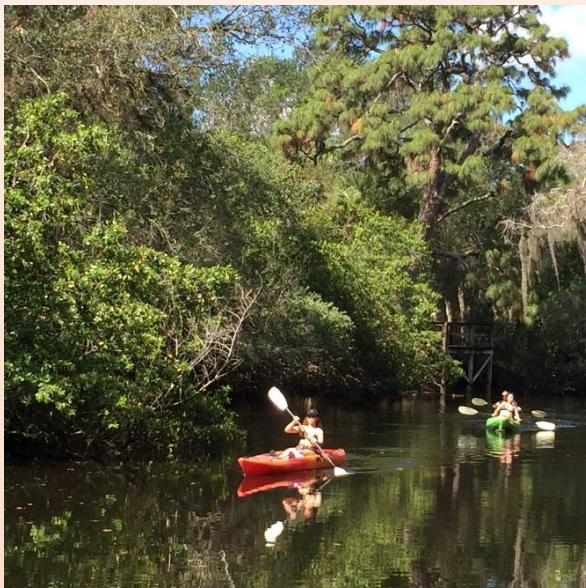
INF-1.7.2 The Village shall work cooperatively with the County's efforts to identify and monitor the disposal activities of hazardous wastes generators and with the County's local program to collect (and properly dispose of) small quantities of hazardous materials such as pesticides, paint, used motor oil, etc.

INF-1.7.3 The Village shall work cooperatively with other agencies to promote community clean up events to promote a clean and litter free community.

CONSERVATION & COASTAL MANAGEMENT

Nature

The Village's unique natural environment consists of a variety of wildlife and plant communities. A natural treasure and focal point of the community is the Estero River and its tributaries, which lead to the Estero Bay, an estuarine body of water at the Village's western boundary. The Village puts priority on protection of natural resources while also supporting access to natural areas for residents and visitors in ways that do not compromise the protection of these resources.



Natural Treasures

The Estero River, Estero Bay Buffer Preserve, Estero Bay, old growth trees, and wildlife are treasured by Estero's residents. It is a priority of the Village to maintain and integrate these resources in the lives of residents in ways that do not compromise them.

Preservation

It is critical to the Village to preserve and maintain conservation lands and natural areas to enrich the native habitats, plants and wildlife. This can be accomplished through active participation with the agencies managing public lands, and through enforcement of maintenance standards for privately managed lands.

Coastal Resources

Coastal resource protection strategies are necessary to reduce the risk of flooding, storm surge and the impacts of sea level rise.

Hazard Protection

The Village aligns with Lee County's emergency management and hazard mitigation plans and strategies to ensure protection of residents and property.

The Village establishes goals, objectives and policies that emphasize preserving, protecting, and integrating natural features within the community in the following ways:

Plant and Tree Preservation

Through coordination with agencies who maintain public lands and establishing standards for maintenance of private landscaping, green spaces, and trees, the Village will ensure plant and tree health and preservation.

Water Access and Quality

The Village will support efforts to develop, regulate and implement programs to counter negative impacts on water from recreational, marine, and development issues while providing public access to the Estero River and supporting the viability of the Calusa Blueway.

Coastal Conservation

By coordinating with county, state and federal agencies to maintain or improve water quality and wildlife diversity, the Village will work toward improving the current pollution loading conditions to support estuarine health.

Wildlife Preservation

Ensuring floral and faunal species diversity, water quality, and natural surface water resources helps to sustain wildlife. The Village will coordinate with agencies who maintain public lands and establish standards for maintenance of privately managed preserves, which helps to support native habitats.

Resource Management and Efficiency

The Village supports smart and innovative use of resources to ensure their sustainability through promoting Florida-friendly ~~and xeriscape~~ landscaping techniques, encouraging the use of bio diesel, electricity and other clean fuels.

Hazard Protection

Through adoption of appropriate development and redevelopment regulations, the Village seeks to reduce the risk of flooding, storm surge, and impacts of sea level rise. Coordination with Lee County's emergency planning and hazard mitigation programming will ensure protection of the public.

CCM-1

GOAL

The Village's unique natural environment consists of wildlife and plant communities, the Estero River and its tributaries leading to the Estero Bay that defines the Village's western boundary. The Village will support and promote public access to greenway and waterfront areas by residents and visitors in ways that do not compromise the protection of these natural resources.

PRESERVE MANAGEMENT COORDINATION

CCM-1.1

OBJECTIVE

The Village has thousands of acres of public and private natural areas that are protected as state parks, public lands, or private preserve areas. These areas offer tremendous ecological, environmental, recreational and community benefits to the Village. The Village will collaborate with public agencies and private developers to preserve and maintain interconnected natural areas to benefit citizens and visitors.

POLICIES

CCM-1.1.1 The Village shall develop and maintain an inventory of public and private parks and conservation areas and other natural areas existing within the Village boundaries.

CCM-1.1.2 Collaborate with public and private land managers to create interconnections between existing and future parks and preserve areas for recreational and wildlife benefits, in ways that do not compromise the protection of natural resources.

CCM-1.1.3 Engage in public-private partnerships that foster the responsible use and enjoyment of parks and preserve areas.

CCM-1.1.4 Encourage responsible land management techniques for public and private natural areas and parks designed to mimic natural South Florida ecosystems, increase biodiversity of beneficial native plant and animal species, and reduce fuel load and the potential for catastrophic wildfires.

CCM-1.1.5 The Village shall facilitate the cooperation of public and private land managers to reduce cost and increase effectiveness of the management of natural areas.

CCM-1.1.6 The Village shall seek to ensure that private developments perform maintenance and protect landscaping, drainage systems and preservation areas in accordance with applicable standards in the land development code and applicable permit conditions.

- A. General trees, buffer trees and plantings that are required as part of development orders should be maintained in a healthy condition. Any change to established general trees, buffer trees and plantings that are required as part of development orders shall be governed by standards and procedures outlined in the land development code that relate to vegetation removal permit requirements and

replacement plans subject to Village approval. New development shall follow the Lee County standards for prohibited and permitted plant species.

- B. Preserves or conservation lands that are required as part of development orders should be maintained in a healthy and exotic free condition. Any vegetation removal or impacts associated with an area designated as a preserve or conservation area is limited per the applicable standards in the land development code, applicable permit conditions, and stipulations associated with conservation easements.
- C. Vegetation removal proposed on wooded or otherwise undeveloped sites is discouraged. Vegetation removal shall be governed by standards and procedures outlined in the land development code.

WATER ACCESS

CCM-1.2

OBJECTIVE

Support efforts to develop, regulate and implement programs to counter negative impacts on water from recreational, marine, and new and existing development, while providing public access and supporting the viability of the Calusa Blueway.



Image by Johnson Engineering, Inc.

POLICIES

CCM-1.2.1 Discourage new public access to the Estero River for motorized watercraft while encouraging new public access to the river for non-motorized and electric watercraft in ways that do not compromise the protection of natural resources.

CCM-1.2.2 The Village will cooperate to the extent practicable with the City of Bonita Springs and Lee County to provide public access to Estero Bay.

CCM-1.2.3 Evaluate feasibility of a scenic waterways program in order to enhance the opportunities for physical, visual, and interpretive access to the water.

CCM-1.2.4 Encourage Seek public private partnerships to encourage private landowners along the Estero River to provide some form of water access to the public.

CCM-1.2.5 The Village will develop strategies for increasing public water access which may include development review, regulation, incentives, and acquisition. The first step in this process will be to develop and maintain an inventory of existing public and private water access sites in the Village.

COASTAL RESOURCE PROTECTION

CCM-1.3

OBJECTIVE

The Village will adopt development and redevelopment principles and strategies that reduce the risk of flooding, storm surge, and impacts of sea level rise.



Image by Johnson Engineering, Inc.

POLICIES

CCM-1.3.1 Public expenditures in areas particularly subject to repeated destruction by hurricanes will be limited to necessary repairs, public safety needs, services to existing residents, and recreation and open space uses

CCM-1.3.2 Conserve and enhance the natural functions of environmentally critical coastal areas.

CCM-1.3.3 Continue to encourage the construction of environmentally compatible shoreline stabilizing systems where stabilizing systems are needed.

CCM-1.3.4 Support efforts to maintain the ecosystem that nourishes and shelters the commercial and sport fisheries surface waters adjacent to the Village

CCM-1.3.5 The Village shall review the Florida Department of Economic Opportunity's "Community Resiliency Initiative: Planning for Adaptation to Sea Level Rise," and the "Lee County Climate Change Resiliency Strategy" (CCRS) as well as other state and federal planning resources, as part of its efforts to establish effective strategies to plan for and adapt to sea level rise. In examining the statewide planning framework, the Village can better determine how best to integrate sea level rise adaptation policies into existing processes.

CCM-1.3.6 The Coastal High Hazard Area, shall be defined as stated in Florida Statute Chapter 163, as the areas below the elevation of the category 1 storm surge line as established by a Sea, Lake, and Overland Surges from Hurricanes (SLOSH) computerized storm surge model, as described in Florida Statute 163.3178(2)(h). The area defined should be based upon the most recently available data published by the Florida division of emergency management.

CCM-1.3.7 Comprehensive plan amendments within the CHHA must meet one of the following criteria in accordance with state statutory requirements in Florida Statute 163.3178(8)(a):

A. The proposed amendment shall demonstrate that a 16-hour out of county hurricane evacuation time for a category 5 storm event, as measured on the Saffir-Simpson scale, is maintained; or

B. The proposed amendment shall demonstrate that a 12-hour evacuation time to shelter for a category 5 storm event is maintained, and ensure adequate shelter space is available to accommodate the additional residents of the development allowed by the proposed amendment; or

C. The proposed amendment shall provide mitigation to satisfy the provisions of evacuation time and shelter space, which may include payment of money, contribution of land, and/or construction of hurricane shelters and/or transportation facilities. The required mitigation shall be addressed in a binding agreement and shall not exceed the amount required to accommodate the impacts reasonably attributable to the development.

CCM-1.3.8⁷ Critical evacuation roadway links shall receive high priority for maintenance and capital improvement expenditures.

CCM-1.3.9⁸ Continue to regulate and encourage proper coastal management techniques through site plan review and zoning mechanisms.

CCM-1.3.10⁹ Implement development and redevelopment engineering solutions and standards that reduce the flood risk in coastal areas and along waterways such as the Estero River and Halfway Creek. Tools to be considered, but not limited to, are:

- Public infrastructure planning, siting, construction, replacement, operation and maintenance
- Stormwater Management
- Green Streets
- Rain Gardens/Bioswales
- Vernacular Streetscapes and Plant Placement (Water-Dependent Species)
- Grass Pavers (Driveways, Main Streets, Ponding Areas)
- Ecological Asset Preservation (Natural Areas, Mangroves, Plants, Animals)
- Land Development Regulations
- Comprehensive Planning

CCM-1.3.11 The Village shall develop a stormwater master plan to readdress recurring flood issues which takes into account an anticipated increase in flooding from rainfall and storm surge.

CCM-1.3.12 All development and major redevelopment in the CHHA shall be consistent with the flood-resistant construction requirement in the Florida Building Code and applicable flood plan management regulations set forth in 44 CFR part 60.

CCM-1.3.13 Development in coastal areas shall consider site development techniques to reduce losses due to flooding and claims made under flood insurance policies issued in the state. These may include accommodation strategies, such as elevating structures,

drainage improvements, or green infrastructure techniques that function to absorb or store water.

CCM-1.3.14 The Village shall limit public expenditures that subsidize development in the CHHA.

NATURAL RESOURCE PROTECTION

CCM-1.4

OBJECTIVE

Protect wetland and upland ecosystems so as to maintain and enhance native habitats, floral and faunal species diversity, water quality, and natural surface water and groundwater resources.



Image by Johnson Engineering, Inc.

POLICIES

CCM-1.4.1 Ensure the long-term protection and enhancement of the natural upland and wetland habitats through the retention of interconnected, functioning, and maintainable hydroecological systems where the remaining wetlands and uplands function as a productive unit resembling the original landscape.

CCM-1.4.2 Conserve habitat of native and non-invasive plant and animal species through development review, regulation, incentives, and acquisition.

CCM-1.4.3 Continue implementing regulations and incentives to prevent incompatible development in and around environmentally sensitive lands; which are rare and unique uplands as described in Policy CCM-1.5.1 and wetlands, as defined in the definition section of this Plan.

~~CCM-1.4.4 Prevent water management and development projects from altering or disrupting the natural function of significant natural systems.~~

CCM-1.4.5 The Village attaches great importance to the integrity of the Density Reduction/Groundwater Resource Area (DR/GR) in so far as actions in the DR/GR area have an impact on the environment, natural resources, mobility, sense of place, and character of Estero. The Village shall advocate to discourage development or future land use changes of DR/GR designated lands if the proposed development or future land use change has potential to increase density, intensity or traffic within the Village

CCM-1.4.6 Require that the integrity of sloughs be maintained and restored, as opportunities arise, so that natural flow ways are functionally preserved.

CCM-1.4.7 Evaluate the possibility of a building setback buffer requirement from the Estero River to protect natural plant communities and the river system.

PLANT COMMUNITIES

CCM-1.5

OBJECTIVE

Preserve and protect the inherent integrity of the Village's natural plant communities.



Image by Johnson Engineering, Inc.

POLICIES

~~CCM-1.5.1 Develop and maintain an inventory of upland habitats that, due to their presence or importance to the Village, should be retained in their natural state and incorporated as amenities into the design of new developments.~~

~~CCM-1.5.2~~ 1 ~~Encourage~~ Establish standards in the Land Development Code for the retention and preservation of ~~high-value upland and wetland habitat types that are unique, rare, or otherwise important to the Village~~ environmentally critical areas including wetlands and Rare and Unique upland habitats. Rare and Unique upland habitats include, but are not limited to: sand scrub (320); coastal scrub (322); those pine flatwoods (411) which can be categorized as "mature" due to the absence of severe impacts caused by logging, drainage, and exotic infestation; slash pine/midstory oak (412); tropical hardwood (426); live oak hammock (427); and cabbage palm hammock (428). The numbered references are to the Florida Land Use Cover and Forms Classification System (FLUCFCS) Level III (FDOT, 1985).

~~CCM-1.5.3~~ 2 ~~Encourage~~ Establish standards in the Land Development Code for the preservation of wetland and upland habitats that are contiguous to other public or private natural areas along with the hydrologic interconnection between such preserve areas.

~~CCM-1.5.4~~ 3 During development review, require applicants to submit environmental inventories and assessments of the impacts of development and encourage the protection and maintenance of viable tracts of sensitive or high-quality natural plant communities within developments.

~~CCM-1.5.5~~ 4 Maintain regulations to minimize and control the clearing of natural vegetation, including tree removal and clearing of understory, prior to the development of property or its conversion to agricultural uses.

~~CCM-1.5.6~~ 5 Development adjacent to aquatic and other nature preserves, wildlife refuges, and recreation areas must protect the natural character and public benefit of these areas including, but not limited to, scenic values, and wildlife habitat for the benefit of future generations.

TREE PROTECTION

CCM-1.6

OBJECTIVE

Protect heritage, historic, and champion trees and forests as unique community resources.



Image by Johnson Engineering, Inc.

POLICIES

CCM-1.6.1 The Village shall develop and maintain an inventory of specimen treesheritage, historic, and champion trees that, due to their uniqueness or importance to the Village, should be retained in their natural state and incorporated as amenities into the design of new developments.

CCM-1.6.2 Evaluate the Land Development Code for opportunities to strengthen mechanisms for the protection of trees associated with any development or redevelopment.

WILDLIFE

CCM-1.7

OBJECTIVE

The Village shall maintain and enhance fish and wildlife diversity and distribution for the benefit of a balanced ecological system.



Image by Johnson Engineering, Inc.

POLICIES

CCM-1.7.1 Support protectingProtect habitats of endangered and threatened species and species of special concern including but not limited to southern bald eagles, west Indian manatees, gopher tortoises, red-cockaded woodpecker, woodstork, Florida panther and black bear, smalltooth sawfish, bonneted bat in order to maintain or enhance existing population numbers and distributions of those species.

CCM-1.7.2 Participate with the Florida Fish and Wildlife Conservation Commission in the development of a regional plan that identifies and protects areas utilized by wildlife, including panthers and bears, so as to promote the continued viability and diversity of regional species.

CCM-1.7.3 Protect wildlife habitat located within areas designated as Wetlands on the Future Land Use Map consistent with the requirements of the Wetland future land use category and Objective CCM-1.8 and Policies CCM-1.8.1, 1.8.2, and 1.8.3.

CCM-1.7.4 Protect wildlife habitat located within areas designated as Conservation on the Future Land Use Map consistent with the requirements of the Conservation future land use category.

CCM-1.7.5 Protect wetland wildlife habitat located outside areas designated as Wetlands or Conservation on the Future Land Use Map consistent with the wetland protection requirements of Objective CCM-1.8 and Policies CCM-1.8.1, 1.8.2, and 1.8.3.

CCM-1.7.6 Protect upland habitat of threatened and endangered species and species of special concern located outside areas designated as Wetlands or Conservation on the Future Land Use Map consistent with Objective CCM-1.7 and Policies CCM-1.7.1 and CCM-1.7.2.

CCM-1.7.7 Establish standards in the Land Development Code that will provide for upland buffers (that maintain the natural habitat function of the buffer area) adjacent to preserved wetlands to provide habitat diversity and protection of wildlife.

WETLANDS

CCM-1.8

OBJECTIVE

The Village recognizes that wetlands are an invaluable natural resource which contribute vital benefits for the public, wildlife, ecosystems and the environment and will maintain and enforce a regulatory program for development in wetlands that is cost-effective, complements federal and state permitting processes, and protects the fragile ecological characteristics of wetland systems.



Image by Johnson Engineering, Inc.

POLICIES

CCM-1.8.1 The natural functions of wetlands and wetland systems will be protected and conserved through the enforcement of the Village's, the State of Florida's, and the US Army Corp of Engineers' wetland protection regulations and the goals, objectives, and policies in this plan at a minimum.

CCM-1.8.2 To the extent allowed by law, the Village will require wetland mitigation for wetlands impacted by development or construction within the Village to be mitigated in order of preference: 1) On the same site or downstream of the impact within the same watershed; 2) within Estero; and 3) as close geographically to Estero as possible but still within the Estero Bay watershed. The Village will coordinate with South Florida Water Management District, Lee County, and the US Army Corps of Engineers to determine the nature and level OF mitigation on a case-by-case basis.

CCM-1.8.3 Encourage upland preservation in and around preserved wetlands to provide habitat diversity, enhance edge effect, and promote wildlife conservation.

WATER QUALITY

CCM-1.9

OBJECTIVE

The Village coordinate with County, state and federal agencies to manage the quality of the Village's surface waters with priority on the Estero Bay ecosystem so as to maintain or improve water quality and wildlife health and diversity; to reduce or maintain current pollution loading and system imbalances in order to conserve estuarine productivity; and to provide the best use of estuarine areas.



Image by Johnson Engineering, Inc.

POLICIES

CCM-1.9.1 The Village shall review, evaluate, and work cooperatively with applicable local, and state, and federal agencies to address all new upland developments in terms of their impacts on estuarine systems.

~~CCM-1.9.2 The Village will work to ensure new development will maximize stormwater retention and treatment.~~

CCM-1.9.23 New development must not degrade water quality of surface waters or groundwater or contribute to flooding of other areas.

CCM-1.9.34 It is hereby declared that in the interests of preserving public health and of preserving and enhancing environmental quality, it is in the public interest to abate and cease use of septic tanks and wastewater treatment package plants where and when central sewer is available. The Village will identify the existing inventory of septic tanks in the Village, evaluate water quality issues associated with them, and coordinate with property owners, local utility providers and Florida Department of Environmental Protection to develop a program, schedule and funding to convert from septic tank use to central sewer facilities. Abatement of the use of private potable water wells and conversion to central water facilities should be considered in conjunction with this program.

CCM-1.9.45 With the cooperation of the respective utility firms or agencies, the Village will coordinate as needed on programs for the abatement of septic tanks and package plants in areas in which sewer is presently available and in areas encompassed by assessment districts established for upgrading sewer availability.

CCM-1.9.56 Through education, incentives, and standards in the land development code, encourage residents, owner associations, and golf courses to utilize responsible fertilizer practices designed and implemented specifically to reduce nutrient runoff into wetlands and waterways.

WATER RESOURCE PROTECTION

CCM-1.10

OBJECTIVE

Support Lee County and Bonita Springs Utilities efforts to provide continued reliable, sustainable, and cost-effective water resource availability.

POLICIES

CCM-1.10.1 Work with Lee County and utility providers to identify water needs consistent with projections of human population and the needs of natural systems in order to determine the future demands for groundwater. The Village will support appropriate measures to ensure water supplies of sufficient quantity and quality to meet the present and projected demands of all consumers and the environment, based on the capacity of the natural systems.

CCM-1.10.2 Comply with the Mandatory Year Round Landscape Irrigation Conservation Measures as detailed in the Florida Administrative Code and cooperate with emergency water conservation measures of the South Florida Water Management District.

CCM-1.10.3 Develop and implement a Water Supply Facilities Work Plan, as required by the State of Florida. The Village shall use the document as the Village's guide to water supply facility planning with a minimum planning horizon as specified by the State of Florida. A copy of the adopted Water Supply Facilities Work Plan will be maintained and kept on file by the Village.

CCM-1.10.4 Promote Florida-friendly ~~and xeriscape~~ landscaping techniques in the land development code and ~~will~~ provide education on water conservation through creative landscaping, and promote the conservation and use of native plant species through ~~xeriscape~~ Florida-friendly landscaping techniques on Village projects.

CCM-1.10.5 Well stimulation treatments in Estero, including acid stimulation and hydraulic and acid fracturing, or "fracking," are prohibited.

CCM-1.10.6 Support and comply with Lee County's wellfield protection ordinance protecting the quality of water flowing into potable water wellfields.

AIR QUALITY

CCM-1.11

OBJECTIVE

Maintain the best possible air quality, meeting or exceeding state and federal air quality standards.



Image by Village Staff

POLICIES

CCM-1.11.1 Encourage the use of bio diesel, electricity and other clean fuels in public vehicles, and consider implementing incentives in the land development code for dedicated parking for such vehicles in new developments and redevelopments.

PLANNING, COORDINATION AND IMPLEMENTATION

CCM-1.12

OBJECTIVE

The Village will adopt and coordinate on the implementation of Lee County's emergency plans and programs designed to protect the public from natural and technological hazards.



Image from estero-fl.gov

POLICIES

CCM-1.12.1 Address disaster preparedness, response and recovery and mitigation by coordinating with and participating in the Lee County Comprehensive Emergency Management Plan, Post-Disaster Strategic Plan, and Flood Plain Management Ordinance.

CCM-1.12.2 In the event of a disaster, the Village shall cooperate with Lee County to assist people with special needs (those who are elderly, frail, infirm, or handicapped) during an emergency.

CCM-1.12.3 The Village shall adhere to Lee County's hazardous material emergency responses plan and will coordinate with the Estero Fire District on the provision of notification, resource, and logistical support to the response team and other primary agencies responding to hazardous material incidents to ensure proper containment, cleanup and disposal of hazardous materials involved in an emergency incident.

CCM-1.12.4 Adhere to Lee County Emergency Evacuation procedures and guidelines to ensure provision of evacuation and shelter capabilities adequate to safeguard the public against the effects of hurricanes and tropical storms.

CCM-1.12.5 Coordinate with Lee County and Estero Fire District to provide planning and decision-making to guide redevelopment during the response and recovery period following major emergencies, such as tropical storms and hurricanes.

CCM-1.12.6 The Village acknowledges that wildfires present a threat to life and property. State parks, public lands, and private preserve areas increase in flammable fuel loads each year and can be ignited by natural and human-induced causes. Through public education efforts, cooperation with state park rangers, review of preserve management plans the Village will promote wildfire defense and resilience strategies of Fire Wise Communities to decrease the risk of damages associated with catastrophic wildfires while increasing the usability and enjoyment of natural areas.

RECREATION & OPEN SPACE

Leisure

One of the most significant goals of the Village is maintaining a variety of recreation areas and open spaces to meet the needs of the residents. High quality recreation opportunities and open spaces are desired by Village residents to provide venues for leisure and greenery within the community.



Estero Community Park

Estero Community Park is located in the heart of the Village. It is a valuable resource for residents of all ages for gathering, play and outdoor enjoyment. The Village will explore opportunities to expand this park as an asset that may serve the Village's need for a signature recreation space and center of civic activities.

Water

Southwest Florida boasts natural waterways that are of recreational value to residents and visitors. Recognizing the significance of the Estero River, the Village seeks to increase recreational potential and access while protecting this resource from any negative impacts of human activity.

Linkages

In the spirit of the community's appreciation for outdoor activity and recreation, the Village will maximize opportunities to develop pathways that link Estero Community Park, Estero Bay Preserve, Koreshan State Park, and the Estero River with destinations near and far.

Coordination

Through a Village wide master planning effort, the Village will identify the strategies and priorities for recreation and open space. This will inform the Village efforts to coordinate with The School District, and county and state agencies that manage Estero Community Park, Estero Bay Preserve, and Koreshan State Park.

One look at the extent of green areas on the Village's Future Land Use Map, and it is apparent that Village residents enjoy a variety of recreation facilities and open spaces. Many assets are in the public domain, and an even greater amount of recreation and open space lies within private communities. The public parks within Estero are currently operated by other agencies. The goals, objectives and policies outlined in this Element demonstrate the Village's commitment to support public park facilities through coordination and expanding recreational opportunities and open spaces for its residents.

Master Planning Effort

A Village-wide master plan will be conducted to assess current conditions and identify and prioritize recreation and open space needs and desires of the community. This master plan will address the needs and desires of all ages, including indoor and outdoor facilities, and the future opportunities related to Estero Community Park.

Interconnections

A common theme of the Village is increasing access and connectivity, especially for outdoor enjoyment. Connecting residents to natural waterways of the Estero River, Estero Bay, and beaches to the west and for recreational purposes is to be balanced with controlling impacts in recognition of their significance and sensitivity. The Village also seeks to connect parks with each other and with destinations via pathways and potential use of the CSX rail corridor and FPL corridor.

Coordination

The Village will continue collaborating with Lee County, the State of Florida, The School District, private developers and other local, state, and national organizations to enhance the existing facilities, including State Parks; increase the opportunities for public access, and encourage additional recreational facilities and services to be provided at those sites in ways that do not compromise the protection of the natural resources.

REC-1

GOAL

Support Estero's quality of life through the development of a broad array of parks, public open spaces, recreational services, recreational facilities, and unique natural environments by collaborating with Lee County, the State of Florida, private developers and other local, state, and national organizations.

RECREATION & OPEN SPACE MASTER PLAN

REC-1.1

OBJECTIVE

The Village has thousands of acres of public and private natural areas that are protected as state parks, public lands, or private preserve areas. These areas offer tremendous ecological, environmental, recreational and community benefits to the Village. The Village will collaborate with public agencies and private developers to preserve and maintain interconnected natural areas to benefit citizens and visitors.



Image by Johnson Engineering, Inc.

POLICIES

REC-1.1.1 Through the master planning process, assess existing park facilities and services and determine if improvements and additions are desired to better serve the residents.

REC-1.1.2 Through the master planning process develop a system to prioritize recreation and open space improvements to meet the needs of the residents.

REC-1.1.3 Through the master planning process, address the need for diversity of recreation and open space opportunities for the Village residents.

REC-1.1.4 Through the master planning process, determine minimum recommended recreational and open space service and facilities guidelines, to prioritize recreation and open space needs in the five-year Capital Improvements Plan.

REC-1.1.5 Through the master planning process, evaluate neighborhood park needs.

ESTERO COMMUNITY PARK

REC-1.2

OBJECTIVE

Evaluate Estero Community Park as a Village asset that may serve the Village's need for a signature recreation space and focal point.



Image by Johnson Engineering, Inc.

POLICIES

REC-1.2.1 Evaluate potential for constructing the originally planned westerly Estero Community Park entrance onto Via Coconut Point.

REC-1.2.2 Evaluate other areas for potential signature recreation spaces.

WATER ACCESS AND THE ESTERO RIVER

REC-1.3

OBJECTIVE

Increase the recreation potential of the Village's natural waterways and recognize the importance of the Estero River as a unique asset



Image by Johnson Engineering, Inc.

POLICIES

REC-1.3.1 Consider implementing policies to increase safety for all motorized and non-motorized water bound vessels.

REC-1.3.2 Seek opportunities to provide boat ramps, and canoe/kayak launches; in appropriate areas.

REC-1.3.3 Evaluate locations, design standards, and implementation techniques for providing public access to appropriate waterways.

REC-1.3.4 Improve public access, use, and enjoyment of Estero's waterfront and water-based resources by supporting the creation of community water-based amenities such as Estero Bay water taxi, marina facilities, or other water-dependent facilities on a limited basis where appropriate.

REC-1.3.5 Encourage new development to provide public access to Estero waterways, as appropriate. Particular emphasis shall be given to properties along Estero River and its tributaries.

REC-1.3.6 Consider requirements or incentives in the land development code for new development and redevelopment to provide water access when developing along or near water bodies.

REC-1.3.7 Pursue grants and other initiatives such as the "Save Our Coast" program as funding sources for additional water accesses.

REC-1.3.8 Develop appropriate public/private and public/non-profit partnerships with land owners, businesses, and organizations that provide recreational opportunities to the public or that maintain open space for recreation or conservation purposes. When feasible, the Village shall encourage and provide assistance to entities that further the recreation and open space goals and objectives of this element, especially those entities that provide public access to the Estero River.

REC-1.3.9 Discourage conversion of water dependent uses to other uses.

INTERCONNECTIONS

REC-1.4

OBJECTIVE

It is a priority of the Village to link areas with a non-motorized travel network, particularly Koreshan State Historic Site, the Boomer property, the Happehatchee Center, Estero Bay Preserve State Park, and Estero Community Park.



Image by Johnson Engineering, Inc.

POLICIES

REC-1.4.1 As part of the recreation & open space master planning process, design a path system of greenways, blueways, and shared-use paths that connect the Estero Community Park, Village Center, Happehatchee Center, Estero Bay Preserve State Park, Koreshan State Historic Site, Boomer Property, Estero River, CSX rail corridor, Edison Farms, Coconut Point Shopping Center, and as many other destinations and residential neighborhoods as possible. This includes seeking opportunities to establish a pedestrian or bicycle path along the Estero River to provide recreation opportunities and interconnection.



Above diagram provided by Estero Council of Community Leaders

REC-1.4.2 Support efforts of the Lee County Metropolitan Planning Organization (MPO) to plan for hiking/biking/walking trails along the CSX rail corridor and other alternative areas.

REC-1.4.3 Seek SUN Trail grant funding to establish the CSX rail corridor as a greenway to provide recreation and open space opportunities and interconnection between residential areas, the Village Center, and other destinations.

REC-1.4.4 Coordinate with the MPO to develop a Bicycle & Pedestrian Master Plan.

REC-1.4.5 Investigate the appropriateness of extending the Estero River blueway to the Village Center as part of the master planning process.

REC-1.4.6 Coordinate trails and greenway planning and construction efforts with private landowners to identify, protect, develop, and manage linear open space connectors for recreation and conservation greenway corridors and encourage private landowners to dedicate greenway facilities for public recreational use through incentives.

REC-1.4.7 New development and redevelopment in areas containing a component of the greenways trail system, as identified by the Recreation & Open Space Master Plan, should

incorporate the greenway trail into their development design, which may count towards the projects' general open space requirements.

REC-1.4.8 Seek opportunities to provide canoe/kayak launches along the blueways intersecting with greenways in ways that do not compromise the protection of natural resources.

REC-1.4.9 Work with private developments to provide blueway, sidewalk, and/or shared-use path linkages through the use of incentives, Land Development Code requirements, and other development tools.

REC-1.4.10 Provide nodes along greenways and multi-use paths, whenever possible. Input from community groups, Lee County, the Metropolitan Planning Organization, and the Florida Department of Transportation may be requested to create land development code guidelines for greenway and multi-use path nodes, which may include the provision of landscaping, stationary fitness equipment, benches, grills, and shade structures. In addition to developer funded nodes, potential funding sources could include public/private partnerships, community involvement programs, and community groups.

ENHANCE STATE PARKS AND LEE COUNTY PRESERVED LAND

REC-1.5

OBJECTIVE

Seek opportunities to improve state parks and Conservation 20/20 preserves that are within, adjacent to, or near the Village boundaries through enhancing the existing facilities, increasing the opportunities for public access, and encouraging additional recreational facilities, programs, and services to be provided at those sites in ways that do not compromise the protection of the natural resources.



Image by Johnson Engineering, Inc.

POLICIES

REC-1.5.1 Support the Koreshan State Historic Site by collaborating with the appropriate agencies to improve pedestrian and bicycle access, historic resources and structures, and community park programs and activities.

REC-1.5.2 Provide recreational opportunities within Estero Bay State Buffer Preserve, Edison Farms, Estero River, and Estero Bay by collaborating with the appropriate local, regional, and state agencies and private property owners to ensure the community's parks, preserves, natural amenities, and open spaces have easy access, parking, trails, and other community amenities in ways that do not compromise the protection of the natural resources.

REC-1.5.3 Coordinate with state agencies to evaluate additional active recreational uses in appropriate areas of state parks and facilities.

NEIGHBORHOOD PARKS AND OPEN SPACE

REC-1.6

OBJECTIVE

Encourage the creation of neighborhood parks, pocket parks, linear parks and open space.



Image by Johnson Engineering, Inc.

POLICIES

REC-1.6.1 Encourage the incorporation of pocket parks, linear parks, neighborhood parks and/or open space in the design of all new developments. Open space can be provided for spaciousness along streets, in private common areas, and in public areas.

REC-1.6.2 Assist existing neighborhoods which seek to create neighborhood parks and/or recreationally useful open space. Assistance can include design guidance and the creation of a municipal service taxing unit.

REC-1.6.3 Investigate alternative funding sources for the provision of neighborhood parks and/or recreationally useful open space, such as public/private partnerships, community involvement programs, community groups, and state, federal, and local grants.

REC-1.6.4 Protect recreation and open space areas from conversion to incompatible public or private uses.

RECREATIONAL SERVICES

REC-1.7

OBJECTIVE

Encourage a variety of recreational programs to be offered to residents.



Image by Johnson Engineering, Inc.

POLICIES

- REC-1.7.1** Notify residents of recreation services provided within Estero to attract and expand participation.
- REC-1.7.2** Encourage and promote fitness programs to encourage healthy lifestyles among residents.
- REC-1.7.3** Encourage guided historical tours on greenways and blueways, and at recreational facilities, open spaces, and historical sites.
- REC-1.7.4** Coordinate with the owners and/or operators of Germain Arena to promote that facility's use for recreational and other community enhancing activities.

SCHOOL BOARD COORDINATION

REC-1.8

OBJECTIVE

Coordinate with the Lee County School Board to co-locate and share facilities and develop mutually beneficial strategies to meet the community's recreation and open space needs.



Image by Johnson Engineering, Inc.

POLICIES

- REC-1.8.1** Work with the School Board to meet community recreation needs.
- REC-1.8.2** Work with the School Board to establish procedures for maintenance of the facilities and properties.

CAPITAL PLANNING

REC-1.9

OBJECTIVE

The Village shall plan and budget to meet current and future recreation and open space needs.

POLICIES

REC-1.9.1 Seek grant funding opportunities for recreation and open space projects, as appropriate.

REC-1.9.2 Land development will be required to bear a proportionate cost of the provision of new or expanded parks required by such development. The Village will continue to coordinate with Lee County on the distribution of impact fees and, as part of the master planning process, the Village shall assess the possibility of charging recreation impact fees for new development and redevelopment within the Village.

REC-1.9.3 The Capital Improvements Program will identify funding for recreation and open space projects, as needed.

INTERGOVERNMENTAL COORDINATION

Management

The continuous growth, success and endurance of Estero will be the result of proper communication and planning within the Village and across the region. The Village will coordinate with all appropriate agencies and other entities implementing plans, services, and programs affecting the Village.



Growth Management

Of utmost importance to Village residents in maintaining and enhancing the character of the community. Coordination with surrounding jurisdictions, the state, and the region is crucial to address any impacts of growth from outside the community.

Transportation Systems

Most of the major roadways that impact the Village are owned and operated by the County or State. Coordination on the conditions and future plans for these corridors is of great importance to maintaining the community character and quality of life in Estero.

Essential Services and Natural Resources

Other agencies provide and manage water, sewer, storm water management, and solid waste services to Estero. Significant parks and natural areas within the Village are owned and operated by the State and other agencies. The Village is committed to coordinating with these agencies to ensure high quality and cost-effective service delivery and protection of resources in keeping with Estero's values.

Educational assets

Coordination with the School District of Lee County and Florida Gulf Coast University (FGCU) are important to meeting needs of the current and future school aged and adult populations.

Estero recognizes that successful implementation of this comprehensive plan, and ultimately the successful delivery of essential services in Estero, is dependent on planning and coordination with other entities.

Government Coordination

The Village will coordinate with all government agencies whose decisions impact the planning of the Village. This includes coordination with the Department of Economic Opportunity, participation in the Southwest Regional Planning Council community workshops regarding regional issues, and participation in the Florida League of Cities.

The Village will coordinate with Lee County Utilities and Bonita Springs Utilities, and other entities as necessary, regarding water and sewer services.

Transportation planning will be coordinated with the Metropolitan Planning Organization (MPO), Lee County, and the Florida Department of Transportation (FDOT), and other entities as necessary.

Water Supply Coordination and Protection

Water supply plan coordination between the Village and the South Florida Water Management District will be conducted to ensure that the Village Comprehensive Plan remains consistent with the District's latest regional water supply plan.

Education Coordination

The Village will coordinate with the School District of Lee County in the planning, development and siting of schools to ensure that their location is consistent with Village growth policies and developed with co-located public recreation facilities and services.

The Village will work with Florida Gulf Coast University (FGCU) to support locating professional and research facilities that will generate economic development within Village limits near the University.

ICE-1

GOAL

Coordinate with all government agencies and other entities implementing plans, services, and programs affecting the Village.

GROWTH MANAGEMENT AND PLANNING COORDINATION

ICE-1.1

OBJECTIVE

The Village shall continue and improve coordination activities among government agencies with planning and impact assessment duties affecting the Village.



Image from estero-fl.gov

POLICIES

ICE-1.1.1 The Village shall coordinate with the Department of Economic Opportunity and other state agencies for planning purposes.

ICE-1.1.2 As needed, participate in the Southwest Regional Planning Council community workshops regarding regional issues such as economic development/redevelopment, emergency evacuation and/or sea level rise.

ICE-1.1.3 The Village's review of local developments shall include coordination with state agencies; as well as adjacent jurisdictions and federal agencies when necessary.

ICE-1.1.4 Participate in the Florida League of Cities to ensure full cooperation in county-wide issues that may involve the Village.

ICE-1.1.5 Evaluate a need for a joint land use agreement with Lee County to better enable Village input concerning proposed unincorporated developments impacting the Village.

ICE-1.1.6 The Village will evaluate the feasibility of a joint planning agreement with Lee County to coordinate on issues and impacts resulting from development east of the Village, particularly along Corkscrew Road and in the Density Reduction/Groundwater Resource areas.

ICE-1.1.7 Pursuant to requirements of the Florida Statutes, the Village shall participate in any intergovernmental dispute resolution process provided by the Southwest Florida Regional Planning Council for the resolution of intergovernmental planning and growth management issues, excluding regulatory matters unless requested by the parties.

ICE-1.1.8 The Village will work with the county and adjacent jurisdictions to develop joint planning areas as needed, per Section 163.3177(6)(h)(1)(a) of the Florida Statutes.

SERVICE COORDINATION

ICE-1.2

OBJECTIVE

Continue and improve coordination with other units of local government and other entities providing utility services.



Image by Johnson Engineering, Inc.

POLICIES

ICE-1.2.1 Coordinate with Lee County Utilities and Bonita Springs Utilities to ensure provision of acceptable levels of potable water service.

ICE-1.2.2 Coordinate with Lee County Utilities and Bonita Springs Utilities to ensure the provision of acceptable levels of sanitary sewer service.

ICE-1.2.3 Evaluate current mechanisms used for intergovernmental coordination and determine the need to increase intergovernmental coordination activities regarding issues involving: emergency preparedness, traffic engineering, fire rescue, police, water and wastewater treatment and administration.

WATER SUPPLY PLAN COORDINATION

ICE-1.3

OBJECTIVE

The Village shall coordinate with the South Florida Water Management District to ensure that the Village Comprehensive Plan remains consistent with the District's latest regional water supply plan.

POLICIES

ICE-1.3.1 Continue to evaluate the latest water supply plans issued by the South Florida Water Management District to ensure consistency in the Village Comprehensive Plan and the Village's Water Supply Facilities Work Plan.

ICE-1.3.2 Coordinate with other government agencies and private suppliers of potable water during the water supply planning process to include the review of land use changes, addressing population projections, and acceptable level of service standards.

PROTECTING NATURAL RESOURCE SYSTEMS

ICE-1.4

OBJECTIVE

The Village shall protect natural resource systems that cross governmental boundaries through intergovernmental coordination.



Image by Village Staff

POLICIES

ICE-1.4.1 Participate with adjacent county agencies and state and federal environmental agencies in regularly conducted natural resource protection intergovernmental coordination meetings.

ICE-1.4.2 Collaborate with the State of Florida to the extent practicable in order to maintain and enhance the Koreshan State Historic Site's natural areas to protect wildlife and plant communities for the public's benefit.

ICE-1.4.3 Participate with other appropriate governments to prepare and implement water management plans, including the Estero Bay Agency on Bay Management, Charlotte Harbor National Estuary Program, the Charlotte Harbor Management Plan, DEP aquatic preserve management plans, water supply plans, and other water resource management plans.

TRANSPORTATION COORDINATION

ICE-1.5

OBJECTIVE

Coordinate transportation activities with the appropriate agencies and governments.



Image by Johnson Engineering, Inc.

POLICIES

ICE-1.5.1 Participate with the Metropolitan Planning Organization (MPO) and the Technical Advisory Committee for matters of coordination in transportation planning and implementation.

ICE-1.5.2 Use interlocal agreements and other appropriate methods of cooperation to ensure proper coordination concerning Lee County and State DOT roadways within the Village.

ICE-1.5.3 The Village will coordinate with the Lee County Port Authority, by way of an interlocal agreement or other means to assure consistency with airport zoning regulations, consistent with Florida Statute Section 333.03(1)(b).

ANNEXATION

ICE-1.6

OBJECTIVE

Adopt an annexation policy that will enhance and preserve the character of the Village.

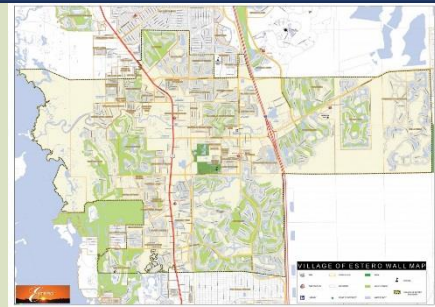


Image from estero-fl.gov

POLICIES

ICE-1.6.1 Approach annexation on a case by case basis considering the following criteria:

- A. Impact on the level or quality of existing services to residents
- B. Impact on the Village Vision
- C. Consistency of the area with the character of the Village
- D. Use of Village services by residents of area to be considered for annexation
- E. Financial benefits and costs

SCHOOL DISTRICT COORDINATION

ICE-1.7

OBJECTIVE

Coordinate with the School District of Lee County in the planning, development and siting of schools within its jurisdiction.



Image by Johnson Engineering, Inc.

POLICIES

ICE-1.7.1 Cooperate with the School District of Lee County to ensure that schools are located consistent with Village growth policies and the needs of the future populations and developed with co-located public recreation facilities and services.

ICE-1.7.2 To the maximum extent possible, the Village shall participate in school facility planning to co-locate public facilities with an emphasis on recreation facilities, libraries, community centers, and other similar facilities with schools.

FLORIDA GULF COAST UNIVERSITY (FGCU) COORDINATION**ICE-1.8****OBJECTIVE**

Work collaboratively with FGCU in its efforts to guide the development of professional, and research and development economic areas in its jurisdiction and near FGCU.

**POLICIES**

ICE-1.8.1 Whenever feasible and appropriate, strive to facilitate the development of professional facilities and research and development facilities within the Village. Particular emphasis shall be placed on locating such facilities in close proximity to FGCU educational resources and advanced technology facilities.

CAPITAL IMPROVEMENTS

Needs

To maintain a high standard of living, the Village will annually analyze existing conditions and available finances to prioritize and implement capital improvements necessary to maintain adopted level of service (LOS) standards.



Five-Year Capital Improvements Program

A five-year capital improvements program will be established to outline funding for capital improvement projects.

Concurrency Management

Level of service standards serve as a basis for facility design and for concurrency management.

Coordination

Capital plans will be coordinated with other service providers and the public school system to ensure capital improvements are consistent with Village interests.

The Village believes in the importance of education to support and enrich a vibrant and attractive community. The public schools in Estero are owned and operated by the School District of Lee County. Other providers include private schools and charter schools. The Village's coordination efforts with these providers will be focused on ensuring schools are located conveniently for its families, and ensuring the schools of the Estero area are recognized for excellence.

Location and Development

The Village will collaborate with the School District of Lee County and representatives of private, charter and parochial school associations to ensure that schools are located consistent with Village growth policies, the future land use map and the needs of the future populations and developed with co-located public recreation facilities and services.

Facilities Coordination

On and off-site improvements associated with existing and new school facilities will be coordinated between the Village and the entity developing the facilities to ensure that schools are neighborhood oriented and well connected to other community facilities.

Services Levels

The Village will work with the School District of Lee County to measure capacity and anticipate needs to ensure adequate facilities are provided to its school aged population. This will be done through the established Lee County public school concurrency management system.

Education and Outreach Programming

The Village supports education to enrich the experiences of students and all citizens through a coordinated education and outreach program. Projects relate to Estero's local environment and local economy are examples of opportunities for residents and students to benefit from Estero's unique assets through an integrated a public education program.

CIE-1

GOAL

To provide public facilities and services adequate to serve the needs of both existing and future development.

FIVE YEAR CAPITAL IMPROVEMENTS PROGRAM

CIE-1.1

OBJECTIVE

Ensure the provision of public facilities at the adopted level of service standards by establishing a capital improvements programming and budgeting system and using the Village's adopted level of service standards as the basic gauge of need and compliance through the Village's concurrency management system and 5-year schedule of capital improvements.



Image by Johnson Engineering, Inc.

POLICIES

CIE-1.1.1 The Village shall annually prepare and adopt an updated five-year Capital Improvements Program showing all public facility development projects to be undertaken during the ensuing five-year period.

CIE-1.1.2 The five-year Capital Improvements Program shall be financially feasible.

CIE-1.1.3 The five-year Capital Improvements Program shall be consistent with all other Comprehensive Plan elements.

CIE-1.1.4 In order to coordinate land uses with available and projected fiscal resources and a financially feasible schedule of capital improvements for water supply and facility projects, the Village shall include in its annual update of five-year capital improvements program the first five years of the ten-year Water Supply Facility Work Plan (WSFWP) to ensure consistency between the Potable Water Sub-Element of the Infrastructure Element and the Capital Improvements Element.

CIE-1.1.5 Capital improvements determined to be necessary to implement the goals, objectives, and policies of this Comprehensive Plan shall be given priority. During the annual update of the five-year Capital Improvements Program, such improvements with costs exceeding \$25,000 and with a life expectancy greater than five years shall be included in the schedule.

CIE-1.1.6 The Village shall annually prioritize funding for recreation and open space land acquisition according to the recommendations of the Estero Recreation & Open Space Master Plan.

CIE-1.1.7 Explore opportunities to identify, prioritize, and fund local capital improvement projects (particularly projects that enhance transportation and infrastructure systems) within the Estero community. Evaluate the feasibility of local, dedicated funding options—e.g.: Municipal Service Benefit Unit (MSBU), Tax Increment Finance District, or other similar mechanism. Capital projects that could be targeted for such funding include:

- A. Streetscape improvements such as roadway pavers, street furniture, street signs and lighting, trash receptacles, and other hardscape features—particularly in Old Estero and within new mixed-use centers;
- B. Pedestrian scale lighting;
- C. Landscaping and hardscape features—particularly along US 41;
- D. Public trails and greenways facilities;
- E. Blue way facilities that provide public access to Estero River;
- F. Multi-modal transportation facilities that expand or establish pedestrian, bike, transit, and rail services;
- G. Public space, park, and recreational facilities;
- H. Urban level infrastructure services and systems within mixed-use center areas; and
- I. Historic resources and facilities such as those associated with the Koreshan Park and Estero Community Park.

CIE-1.1.8 Seek all realistic grant opportunities to fund projects in the Five-year Capital Improvements Schedule.

LEVELS OF SERVICE

CIE-1.2

OBJECTIVE

Level of service (LOS) standards will be the basis for planning the provision of required public facilities within the Village. Some of these standards will be the basis for determining the adequacy of public facilities for the purposes of permitting new development. The level of service standards will be the basis for facility design, for setting impact fees, and (where applicable) for concurrency management.



Image by Johnson Engineering, Inc.

POLICIES

CIE-1.2.1 Land use decisions shall be made based upon available or projected fiscal resources in coordination with a five-year capital improvements program which maintains adopted level of service standards and meets existing and future facility needs.

CIE-1.2.2 Provide for the availability of public facilities and services needed to support development concurrent with the impacts of such development.

CIE-1.2.3 The minimum acceptable level of service standard for potable water connections to Lee County Utilities will be:

An available supply and treatment capacity of 250 gallons per day per equivalent residential connection (ERC), except that facilities serving only multi-family or mobile home residential structures must have a capacity of 200 gallons per day and facilities serving only recreational vehicle or travel trailer residential structures must have a capacity of 100 gallons per day.

CIE-1.2.4 The minimum acceptable level of service standard for potable water connections to Bonita Springs Utilities will be:

An available supply and treatment capacity of 235 gallons per day per equivalent residential connection (ERC) and delivery of potable water at a minimum pressure of 20 pounds per square inch (psi) at the meter anywhere in the system.

CIE-1.2.5 The minimum acceptable level of service standard for sanitary sewer connections to Lee County Utilities is established for the basic facility capacity, which is the principal productive capital of a water or sewer system, i.e., a wellfield and water treatment plant, as distinguished from the distribution system. The minimum acceptable level of service for Lee County Utilities sanitary sewer will be:

Available basic facility capacity to treat and dispose of a volume of wastewater equal to 200 gallons per day per equivalent residential connection (ERC), except that facilities serving only multifamily or mobile home residential structures shall have a capacity of 160 gallons per day and facilities serving only recreational vehicles or travel trailer residential structures must have a capacity of 80 gallons per day.

CIE-1.2.6 The minimum acceptable level-of-service standards for sanitary sewer connections to Bonita Springs Utilities will be:

Available capacity to treat and dispose of a volume of wastewater equal to 200 gallons per day per equivalent residential connection (ERC), except that facilities serving only mobile home residential structures shall have a capacity of 150 gallons per day and facilities serving only travel trailer residential structures shall have a capacity of 120 gallons per day.

CIE-1.2.7 The following surface water management standards are adopted as minimum acceptable levels of service:

- A. The existing surface water management system in the Village will be sufficient to prevent the flooding of designated evacuation routes (U.S. 41, Corkscrew Road, Three Oaks Parkway, Interstate 75, and Ben Hill Griffin Parkway) from the 25-year, 3-day storm event (rainfall) for more than 24 hours.

All new private and public and private infrastructure shall be constructed with floor slabs constructed a minimum of one (1) foot above the 100-year, 3-day storm event flood plain level and shall be safe from flooding from a 100-year, 3-day storm event (rainfall).

- B. Regulation of Private and Public Development

Surface water management systems in new private and public developments (excluding widening of existing roads) must be designed to SFWMD standards (to detain or retain excess stormwater to match the predevelopment discharge rate for the 25-year, 3-day storm event [rainfall]).

Stormwater discharges from development must meet relevant water quality and surface water management standards as set forth in state rules including but not limited to requirements listed in Numeric Nutrient Criteria and Total Maximum Daily Loads and Basin Management Action Plans.

New developments must be designed to avoid increased flooding of surrounding areas ~~and be consistent with the adopted recommendations of the Village Stormwater Master Plan~~. Development must be designed to minimize increases of discharge to public water management infrastructure (or to evapotranspiration) that exceed historic rates, to approximate the natural surface water systems in terms of rate, hydroperiod, basin and quality, and to eliminate the disruption of wetlands and flow-ways, whose preservation is deemed in the public interest.

- C. Design trunk conveyance crossings of arterial roads to be free of flooding from 25-year, 3-day storm events at a minimum.
- D. Other standards may be added by amendment to this comprehensive plan for development impacting water bodies that have water quality impairments identified by Florida Department of Environmental Protection, including but not limited to the following water bodies: 3258C4 - Mullock Creek (Marine Segment); 3258C - Mullock Creek; 3258D1 - Estero River (Marine Segment); 3258D2 - Estero River; 3258H2 - Spring Creek (Marine Segment).

CIE-1.2.8 The minimum acceptable level-of-service standard for availability of solid waste disposal facilities is 7 pounds per capita per day.

CIE-1.2.9 Minimum acceptable motorized vehicle Level of Service (LOS) Standards for roadways as established by the state of Florida and Lee County are recognized as shown below:

I-75	D
Arterials	E
Collectors	E

CIE-1.2.10 The Village adopts the following LOS standards for public schools, based upon Permanent Florida Inventory School Houses (FISH) capacity.

- A. Elementary Schools: 100% of Permanent FISH Capacity as adjusted by the School Board annually to account for measurable programmatic changes.
- B. Middle Schools: 100% of Permanent FISH Capacity as adjusted by the School Board annually to account for measurable programmatic changes.
- C. High Schools: 100% of Permanent FISH Capacity as adjusted by the School Board annually to account for measurable programmatic changes.
- D. Special Purpose Facilities: 100% of Permanent FISH Capacity as adjusted by the School Board annually to account for measurable programmatic changes.

CONCURRENCY MANAGEMENT

CIE-1.3

OBJECTIVE

The Village shall ensure capital revenues and/or secured developer commitments are in place to maintain all public facilities at acceptable level of service standards prior to the issuance of new development orders. Maximize fiscal resources available to the Village for public facility improvements necessary to accommodate existing development, redevelopment, and planned future growth, and to replace obsolete or deteriorated facilities.

POLICIES

CIE-1.3.1 The Village shall coordinate proportionate fair share mitigation procedures and payments with Lee County, the Florida Department of Transportation, and the Lee County School District, where applicable.

CIE-1.3.2 Coordinate planning for the Village improvements with the plans of state agencies, the South Florida Water Management District (SFWMD), Lee County, [Lee County Metropolitan Planning Organization \(MPO\)](#) and adjacent municipalities when applicable.

CIE-1.3.3 Repair, rehabilitate, and replace the Village's capital facilities according to generally accepted engineering principles and guidelines and ensure that facilities and services provided by other agencies are held to the same standard.

CIE-1.3.4 The Village shall follow the following timing requirements to ensure that adequate public facilities are available to meet level of service standards with the impact of development.

- A Sanitary sewer, solid waste, drainage, adequate water supplies, and potable water facilities shall be in place and available to serve new development no later than the issuance by the local government of a certificate of occupancy or its functional equivalent. Prior to approval of a building permit or its functional equivalent, the Village shall determine whether adequate water supplies to serve the new development will be available no later than the anticipated date of issuance by the Village of a certificate of occupancy or its functional equivalent.
- B. Transportation facilities needed to serve new development shall be in place or under actual construction within 3 years after the Village approves a building permit that results in traffic generation.

CIE-1.3.5 Utilize a variety of funding sources to implement capital improvements, within the limitation of existing law. These methods may include ad valorem taxes, general revenues, enterprise revenues, assessments, tax increment, grants, and private funds.

CIE-1.3.6 Ensure that new development bears a proportionate cost for public facility improvements by utilizing a variety of mechanisms to assess and collect impact fees, dedications and/or contributions from private development.

CIE-1.3.7 ~~Decisions regarding land use planning and the issuance of~~ Development orders and permits ~~consider the availability of~~ will not be issued unless the required public facilities and services (transportation, schools, water, sewer, drainage, and solid waste) necessary to support such development at the adopted LOS standards are available concurrent with the associated impacts.

CIE-1.3.8 Coordinate with road, utility and infrastructure service providers within the Village to ensure that necessary capital improvements are implemented to support new construction and redevelopment.

SCHOOL COORDINATION

CIE-1.4

OBJECTIVE

Coordinate public school capital improvements with the school district through an interlocal agreement.

POLICIES

CIE-1.4.1 Assess new development a proportionate fair-share of the public facility costs necessary to accommodate the impacts of new development at the adopted levels-of-service through the enforcement of existing public facility funding mechanisms and impact fees. Public facilities include potable water, sanitary sewer, solid waste, drainage, parks, schools and roadways.

CIE-1.4.2 Capital improvements associated with the construction of educational facilities are the responsibility of the Lee County School District, as well as addressing financial feasibility associated with school concurrency. The Village shall update its Capital Improvements Element and Program annually, to include the annual update of the School District Five-Year Plan and Capital Budget.

CIE-1.4.3 For public school facilities, a proportionate share mitigation agreement is subject to approval by Lee County School District and the Village and shall be (for each development) executed as authorized by the overall master school interlocal agreement.~~included in an interlocal agreement between the School District and the Village.~~

Capital Improvement Schedule

Project #	Project Name	Estimated Project Cost	Funding Source	CIP Budget FY 17-18	CIP Budget FY 18-19	CIP Budget FY 19-20	CIP Budget FY 20-21	CIP Budget FY 21-22	CIP Budget FY 22-23	6 Year Total Cost
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Transportation										
	Estero Pkwy Road Resurfacing ³	\$ 1,019,204.00		\$ 1,019,204.00						\$ 1,019,204.00
	Estero Pkwy Landscaping ¹	\$ 2,677,290.00	General Fund		\$ 2,677,290.00					\$ 2,677,290.00
	Estero Pkwy Bicycle & Pedestrian ¹	\$ 872,570.00		\$ 872,570.00						\$ 872,570.00
	Estero Pkwy Lighting ²	\$ 1,298,066.08		\$ 1,298,066.08						\$ 1,298,066.08
	Broadway Ave Phase 2 Design (Intersection of Broadway Avenue and US-41 turn lane widening) ⁴	\$ 50,000.00		\$ 50,000.00						\$ 50,000.00
	Broadway Ave Phase 2 Design (Intersection of Broadway Avenue and US-41 drainage improvements) ⁴	\$ 50,000.00	General Fund	50,000.00						\$ 50,000.00
	Broadway Ave Road Resurfacing ³	\$ 483,851.25			\$ 483,851.25					\$ 483,851.25
	Williams Rd Phase 2 Design (Intersection of Williams Road/Unnamed Road traffic concerns) ⁴	\$ 50,000.00		\$ 50,000.00						\$ 50,000.00
	Williams Rd Phase 2 Design (west of US-41) ⁴	\$ 50,000.00		50,000.00						\$ 50,000.00
	Williams Rd Phase 3 Design (west of US-41) ⁴	\$ 150,000.00	General Fund		\$ 150,000.00					\$ 150,000.00
	Williams Rd Road Resurfacing and Widening ¹	\$ 2,000,000.00				\$ 2,000,000.00				\$ 2,000,000.00
	Williams Rd Lighting ²	\$ 1,839,545.07				\$ 1,839,545.07				\$ 1,839,545.07
	Sandy Ln Phase 2 Design (Bridge Assessment) ⁴	\$ 50,000.00		\$ 50,000.00						\$ 50,000.00
	Sandy Ln Phase 2 Design (Estero River to Corkscrew Road Drainage Improvements and Landscaping) ⁴	\$ 50,000.00	General Fund		\$ 50,000.00					\$ 50,000.00
	Sandy Ln Landscaping ¹	\$ 35,560.00				\$ 35,560.00				\$ 35,560.00
	River Ranch Rd Phase 2 Design (resurfacing, minor lane widening, sidewalk completion, & drainage improvements)	\$ 50,000.00	General Fund		\$ 50,000.00					\$ 50,000.00
	NW Cnr of Broadway Ave & US41 Phase 2 Design (resurfacing and drainage improvements) ⁴	\$ 50,000.00		\$ 50,000.00						\$ 50,000.00
	NW Cnr of Broadway Ave & US41 Poinciana Avenue Resurfacing ³	\$ 20,250.00	General Fund		\$ 20,250.00					\$ 20,250.00
	NW Cnr of Broadway Ave & US41 Trailside Drive Resurfacing ³	\$ 88,605.00			\$ 88,605.00					\$ 88,605.00
	Interlocal Agreement Ben Hill Griffin Parkway ¹	\$ 750,000.00		\$ 750,000.00						\$ 750,000.00
	Interlocal Agreement Three Oaks Parkway ¹	\$ 750,500.00		750,500.00						\$ 750,500.00
	General Resurfacing Projects ⁵	\$ 4,000,000.00					\$ 400,000.00	\$ 600,000.00	\$ 600,000.00	\$ 1,600,000.00
	General Landscaping and Beautification Projects ⁵	\$ 4,000,000.00					\$ 400,000.00	\$ 600,000.00	\$ 600,000.00	\$ 1,600,000.00
	General Traffic and Safety Projects ⁵	\$ 2,000,000.00	General Fund				\$ 300,000.00	\$ 300,000.00	\$ 300,000.00	\$ 900,000.00
	General Bicycle and Pedestrian Projects ⁵	\$ 1,000,000.00					\$ 400,000.00	\$ 100,000.00	\$ 100,000.00	\$ 600,000.00
	Bicycle and Pedestrian Master Plan	\$ 100,000.00		\$ 50,000.00	\$ 50,000.00					\$ 100,000.00
	Total	\$ 23,485,441.40		\$ 5,040,340.08	\$ 3,569,960.25	\$ 3,875,105.07	\$ 1,500,000.00	\$ 1,600,000.00	\$ 1,600,000.00	\$ 17,185,441.40

Stormwater and Drainage										
	General Stormwater and Drainage Projects ⁵	\$ 2,000,000.00				\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 1,000,000.00
	Broadway Ave Stormwater & Drainage ¹	\$ 531,625.05	General Fund		\$ 531,625.05					\$ 531,625.05
	Stormwater Master Plan	\$ 300,000.00		\$ 150,000.00	\$ 150,000.00					\$ 300,000.00
	Total	\$ 2,831,625.05		\$ 150,000.00	\$ 681,625.05	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 1,831,625.05

Parks and Recreation										
	General Parks and Recreation Projects ⁵	\$ 2,000,000.00				\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 1,000,000.00
	Parks and Recreation Master Plan	\$ 100,000.00	General Fund	\$ 50,000.00	\$ 50,000.00					\$ 100,000.00
	Total	\$ 2,100,000.00		\$ 50,000.00	\$ 50,000.00	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 250,000.00	\$ 1,100,000.00

Total										
	All Projects Combined	\$ 28,417,066.45		\$ 5,240,340.08	\$ 4,301,621.30	\$ 4,375,105.07	\$ 2,000,000.00	\$ 2,100,000.00	\$ 2,100,000.00	\$ 20,117,066.45

¹ Estimates are based upon Tetra Tech-provided project cost estimates

² Estimates are based on per-light costs incurred on the Tamiami Trail (US-41) lighting project, currently being completed by Florida Department of Transportation (FDOT)

³ Estimates are based on the resurfacing cost estimate previously provided by Lee County, which stood at \$7.50 per square yard (SY) of road surface, plus an initial multiplier (understanding Lee County's potentially lower costs due to economies of scale)

⁴ Estimates derived from general assumptions for typical engineering projects of this size and scope

⁵ Figures represent potential annual allocations to specific project categories

Capital Improvement Schedule											
Project Name		Entity	Estimated Current Project Cost	Funding Source	CIP Budget FY 18-19	CIP Budget FY 19-20	CIP Budget FY 20-21	CIP Budget FY 21-22	CIP Budget FY 22-23	CIP Budget Future	Total Cost
1 Roadway Projects											
2	Broadway W. Ave Improvements (US 41-West)	Estero	\$ 737,500	GT		\$ 120,820	\$ 651,210	\$ -	\$ -	\$ -	\$ 772,030
3	Estero Parkway Improvements (US41-Three Oaks)	Estero	\$ 9,300,000	GF	\$ 6,004,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 6,304,000
4				GT	\$ 800,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 800,000
5				LDOT	\$ 2,196,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 2,196,000
6	Williams Road Widening (US41 - Via Coconut)	Estero	\$ 2,162,000	RIF		\$ -	\$ -	\$ 344,420	\$ 2,045,850	\$ -	\$ 2,390,270
7	River Ranch Road Improvements	Estero	\$ 785,000	GF		\$ 122,570	\$ 699,300		\$ -	\$ -	\$ 821,870
8	Corkscrew Road Widening (Ben Hill to Bella Terra)	LDOT	\$ 14,930,000	LDOT		\$ 1,537,790	\$ 7,054,430	\$ 7,255,980	\$ -	\$ -	\$ 15,848,200
9 Intersection Improvements											
10	Coconut Road Roundabout (west of US-41)	Estero	\$ 763,600	GT		\$ -	\$ -	\$ -	\$ -	\$ 870,510	\$ 870,510
11	Williams Road west of US41 Intersection Improvements (Walgreens)	Estero	\$ 642,900	GT	\$ 104,400	\$ 554,660	\$ -	\$ -	\$ -	\$ -	\$ 659,060
12	Williams Road Estero High Turn Lanes	Estero	\$ 381,500	GT	\$ 74,400	\$ 316,321	\$ -	\$ -	\$ -	\$ -	\$ 390,721
13	North Point Railroad Crossing	Estero	\$ 1,000,000	GT		\$ -	\$ -	\$ -	\$ -	\$ 1,140,000	\$ 1,140,000
14	Coconut Road /US-41 Intersection Improvements	Estero / LDOT / Bonita Springs	\$ 379,200	LDOT		\$ 23,320	\$ 108,960	\$ -	\$ -	\$ -	\$ 132,280
15				Bonita Springs		\$ 23,320	\$ 108,960	\$ -	\$ -	\$ -	\$ 132,280
16				GT		\$ 23,320	\$ 108,960	\$ -	\$ -	\$ -	\$ 132,280
17	Corkscrew-I75 Interchange Improvements	FDOT	\$ 12,765,730	FDOT		\$ 13,148,710	\$ -	\$ -	\$ -	\$ -	\$ 13,148,710
18	Corkscrew-US 41 Intersection Improvements	LDOT & FDOT	\$ 273,600	LDOT & FDOT		\$ -	\$ -	\$ -	\$ -	\$ 311,910	\$ 311,910
19	Corkscrew-Cypress Shadows Blvd. Intersection Improvements	LDOT	\$ 692,600	LDOT		\$ -	\$ -	\$ -	\$ -	\$ 789,570	\$ 789,570
20	Corkscrew-Corkscrew Woodland Blvd. Intersection Improvements	LDOT	\$ 27,000	LDOT		\$ -	\$ -	\$ -	\$ -	\$ 30,780	\$ 30,780
21	Corkscrew-Three Oaks Pkwy. Intersection Improvements	LDOT	\$ 132,200	LDOT		\$ -	\$ -	\$ -	\$ -	\$ 150,710	\$ 150,710
22	Ben Hill Griffin Pkwy-Corkscrew Rd. Intersection Improvements	LDOT	\$ 91,400	LDOT		\$ -	\$ -	\$ -	\$ -	\$ 104,200	\$ 104,200
23	Ben Hill Griffin Pkwy at Grande Oaks Blvd Signal	LDOT	\$ 473,900	LDOT	\$ 423,900	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 423,900
24		Estero		GF	\$ 50,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 50,000
25	Corkscrew Rd-Estero Town Commons Signal	Developers	\$ 421,300	Developers		\$ 433,940	\$ -	\$ -	\$ -	\$ -	\$ 433,940
26	US41-Pelican Sound Intersection Improvements	North Point DRI	\$ 556,200	Developers		\$ -	\$ -	\$ -	\$ -	\$ 634,070	\$ 634,070
27	US41-Williams Rd Intersection Improvements (after Williams Widening)	North Point DRI	\$ 14,900	Developers		\$ -	\$ -	\$ -	\$ -	\$ 16,990	\$ 16,990

Project Name			Entity	Estimated Current Project Cost	Funding Source	CIP Budget FY 18-19	CIP Budget FY 19-20	CIP Budget FY 20-21	CIP Budget FY 21-22	CIP Budget FY 22-23	CIP Budget Future	Total Cost
28 Bike-Pedestrian Projects												
29		Coconut Road Crosswalks	Estero	\$ 16,400	GF	\$ 16,400	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 16,400
30		Via Coconut Point Roundabouts Bicycle improvements	Estero	\$ 290,300	GF		\$ -	\$ -	\$ 46,230	\$ 274,730	\$ -	\$ 320,960
31		Corkscrew Palms Blvd. Sidewalk	Lee County	\$ 24,900	Lee County		\$ 25,650	\$ -	\$ -	\$ -	\$ -	\$ 25,650
32		Bicycle & Pedestrian Master Plan	MPO	\$ 100,000	MPO	\$ 100,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 100,000
33 Landscaping Projects												
34		US41 Shoulder Landscaping	Estero	\$ 3,263,700	GF		\$ -	\$ -	\$ -	\$ 299,700	\$ 3,412,820	\$ 3,712,520
35		Corkscrew Road Landscape Enhancement (US-41 to I-75)	Estero	\$ 1,854,800	GF	\$ 144,000	\$ 1,762,130	\$ -	\$ -	\$ -	\$ -	\$ 1,906,130
36		Via Coconut Point Landscape Enhancement	Estero	\$ 2,899,600	GF		\$ -	\$ -	\$ 308,880	\$ -	\$ 2,979,510	\$ 3,288,390
37		Three Oaks Parkway Landscape Enhancement (Excluding Brooks)	Estero	\$ 2,618,700	GF		\$ -	\$ 254,840	\$ 2,566,080	\$ -	\$ -	\$ 2,820,920
38		Ben Hill Griffin Pkwy Landscape Enhancement	Estero	\$ 1,047,400	GF		\$ -	\$ -	\$ -	\$ 107,670	\$ 1,083,460	\$ 1,191,130
39		Monument Sign/Estero Branding	Estero	\$ 96,000	GF	\$ 96,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 96,000
40		US-41 Monument Signs	Estero	\$ 144,000	GF	\$ 144,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 144,000
41		I-75 Monument Signs	Estero	\$ 210,000	GF		\$ -	\$ 44,100	\$ -	\$ 186,480	\$ -	\$ 230,580
42		Three Oaks Parkway Monument Signs	Estero	\$ 138,000	GF	\$ 138,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 138,000
43		Ben Hill Griffin Monument Sign	Estero	\$ 72,000	GF		\$ 74,160	\$ -	\$ -	\$ -	\$ -	\$ 74,160
44		Corkscrew Road Monument Sign	Estero	\$ 72,000	GF		\$ -	\$ -	\$ -	\$ -	\$ 82,080	\$ 82,080
45		US41 Median Landscaping	FDOT/Estero	\$ 1,165,500	FDOT	\$ 766,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 766,000
46					GF	\$ 249,500	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 399,500
47		Corkscrew-I75 Interchange Landscaping	FDOT	\$ 7,650,000	FDOT		\$ -	\$ -	\$ 8,262,000	\$ -	\$ -	\$ 8,262,000
48 Parks & Recreation												
49		Parks & Recreation Master Plan	Estero	\$ 100,000	GF	\$ 100,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 100,000
50		Koreshan State Park Boat Ramp Improvements	Estero	\$ 300,000	GF		\$ 309,000	\$ -	\$ -	\$ -	\$ -	\$ 309,000
51 Building Projects												
52		Public Works Storage Facility	Estero	\$ 120,000	GT		\$ 123,600	\$ -	\$ -	\$ -	\$ -	\$ 123,600

Project Name			Entity	Estimated Current Project Cost	Funding Source	CIP Budget FY 18-19	CIP Budget FY 19-20	CIP Budget FY 20-21	CIP Budget FY 21-22	CIP Budget FY 22-23	CIP Budget Future	Total Cost
53 Land Acquisition												
54		Land Purchase A	Estero	\$ 26,000,000	GF	\$ 5,000,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 5,000,000
55					Debt	\$ 21,000,000	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 21,000,000
56		Land Purchase B	Estero	\$ 12,000,000	GF		\$ -	\$ 3,150,000	\$ -	\$ -	\$ -	\$ 3,150,000
57					Debt		\$ -	\$ 9,450,000	\$ -	\$ -	\$ -	\$ 9,450,000
58		River Oaks Preserve	Estero	\$ 900,000	Unfunded		\$ -	\$ -	\$ -	\$ -	\$ 1,026,000	\$ 1,026,000
59		Pinewood Property (along Corkscrew Rd)	Estero	\$ 4,000,000	Unfunded		\$ -	\$ -	\$ -	\$ -	\$ 4,560,000	\$ 4,560,000
60		Estero High School/Community Park Parcel	Estero	\$ 2,000,000	Unfunded		\$ -	\$ -	\$ -	\$ -	\$ 2,280,000	\$ 2,280,000
				-		-	-	-	-	-	-	-
			Total Cost	<u>\$ 113,613,830</u>		<u>\$ 37,406,600</u>	<u>\$ 18,599,311</u>	<u>\$ 21,630,760</u>	<u>\$ 18,783,590</u>	<u>\$ 2,914,430</u>	<u>\$ 19,472,610</u>	<u>\$ 119,257,301</u>

Allocated Funds:	Allocated Fund														
RIF=Road Impact Fees	RIF	\$	-	\$	-	\$	-	\$	344,420	\$	2,045,850	\$	-	\$	2,390,270
GT=Gas Tax	GT		978,800		1,138,721		760,170		-		-		2,010,510		4,888,201
GF=General Fund	GF		11,941,900		2,267,860		4,148,240		2,921,190		868,580		7,557,870		30,155,640
Estero Total Funds	VOE Total		12,920,700		3,406,581		4,908,410		3,265,610		2,914,430		9,568,380		37,434,111
Estero Debt Proceeds			21,000,000		-		9,450,000		-		-		-		30,450,000
Estero Total including Debt		\$	33,920,700	\$	3,406,581	\$	14,358,410	\$	3,265,610	\$	2,914,430	\$	9,568,380	\$	67,884,111
Estero Unfunded			-		-		-		-		-		7,866,000		7,866,000
Bonita Springs			-		23,320		108,960		-		-		-		132,280
FDOT	FDOT		766,000		13,148,710		-		8,262,000		-		-		22,176,710
LDOT/Lee County	LDOT		2,619,900		1,586,760		7,163,390		7,255,980		-		1,387,170		20,013,200
Developer/Others			100,000		433,940		-		-		-		651,060		1,185,000
Total Project Cost		\$	37,406,600	\$	18,599,311	\$	21,630,760	\$	18,783,590	\$	2,914,430	\$	19,472,610	\$	119,257,301

CAPITAL IMPROVEMENTS

Needs

Lee County School District Summary of Revenue/Expenditures Available for New Construction and Remodeling Projects Only						
	2016 - 17	2017 - 18	2018 - 19	2019 - 20	2020 - 21	Five Year Total
Total Revenues	\$33,000,000	\$67,297,586	\$29,500,000	\$0	\$0	\$129,797,586
Total Project Costs	\$33,000,000	\$67,297,586	\$29,500,000	\$0	\$0	\$129,797,586
Difference (Remaining Funds)	\$0	\$0	\$0	\$0	\$0	\$0
Source: Lee County School District 2016 - 2017 Work Plan						

Lee County School District Capacity Project Schedules									
Project Description			2016-17	2017-18	2018 - 19	2019 - 20	2020 - 21	Total	Funded
New High School in South Zone	Location Not Specified	Planned Cost	\$33,000,000	\$37,797,586	\$0	\$0	\$0	\$70,797,586	Yes
	Student Stations		0	2,103	0	0	0	2,103	
	Total Classrooms		0	78	0	0	0	78	
	Gross Sq Ft		0	375,407	0	0	0	375,407	
New High School in East Zone	Location Not Specified	Planned Cost	\$0	\$29,500,000	\$29,500,000	\$0	\$0	\$59,000,000	Yes
	Student Stations		0	0	2,103	0	0	2,103	
	Total Classrooms		0	0	78	0	0	78	
	Gross Sq Ft		0	0	375,407	0	0	375,407	
District Totals	Planned Cost		\$33,000,000	\$67,297,586	\$29,500,000	\$0	\$0	\$129,797,586	
	Student Stations		0	2,103	2,103	0	0	4,206	
	Total Classrooms		0	78	78	0	0	156	
	Gross Sq Ft		0	375,407	375,407	0	0	750,814	
Source: Lee County School District 2016 - 2017 Work Plan									

PUBLIC SCHOOL FACILITIES

Education

Public schools are an integral part of the community. Through proper planning, public schools can be situated in the Village as places that nurture, educate and prepare the youth of Estero and provide facilities that benefit Village residents of all ages as education is key to a vibrant community.



Facility Planning

The Village seeks to encourage opportunities for school children of the Village to attend schools located in the Village, and to provide co-location of recreation opportunities at school sites.

Capacity

The Village has adapted the concurrency management system established by The School District of Lee County to ensure adequate capacity of the schools to meet local needs.

Ongoing Education

The Village supports strategies to educate and help promote the Village as a clean, vibrant, healthy, and sustainability-minded community. School attendees and the general population can all benefit from a top notch education system.

The Village believes in the importance of education to support and enrich a vibrant and attractive community. The public schools in Estero are owned and operated by the School District of Lee County. Other providers include private schools and charter schools. The Village's coordination efforts with these providers will be focused on ensuring schools are located conveniently for its families, and ensuring the schools of the Estero area are recognized for excellence.

Location and Development

The Village will collaborate with the School District of Lee County and representatives of private, charter and parochial school associations to ensure that schools are located consistent with Village growth policies, the future land use map and the needs of the future populations and developed with co-located public recreation facilities and services.

Facilities Coordination

On and off-site improvements associated with existing and new school facilities will be coordinated between the Village and the entity developing the facilities to ensure that schools are neighborhood oriented and well connected to other community facilities.

Services Levels

The Village will work with the School District of Lee County to measure capacity and anticipate needs to ensure adequate facilities are provided to its school aged population. This will be done through the established Lee County public school concurrency management system.

Education and Outreach Programming

The Village supports education to enrich the experiences of students and all citizens through a coordinated education and outreach program. Projects relate to Estero's local environment and local economy are examples of opportunities for residents and students to benefit from Estero's unique assets through an integrated a public education program.

PSF-1

GOAL

Assist the School District of Lee County and other providers of education (where appropriate) with the planning, development and siting of schools to: promote co-location of facilities that benefit Village residents, promote opportunities for school children of the Village to attend schools located in the Village, and foster a quality of education in and around Estero that enhances the Village's desirability and economy.

SCHOOL LOCATION AND CO-LOCATION PLANNING

PSF-1.1

OBJECTIVE

Cooperate with The School District of Lee County and representatives of private, charter and parochial school associations to ensure that schools are located consistent with Village growth policies and the needs of the future populations and developed with co-located public recreation facilities and services.

POLICIES

PSF-1.1.1 Work in collaboration with The School District of Lee County, representatives of private, charter and parochial school associations, and other interested institutions, for the location and development of educational facilities consistent with Florida Statute Chapter 235, and the policies of this plan.

PSF-1.1.2 All educational institutions will comply with the policies of this plan and the Land Development Code where not pre-empted by state statutes or administrative rules.

PSF-1.1.3 To the maximum extent possible, the Village will participate in school facility planning to co-locate public facilities, with an emphasis on recreation facilities, and also libraries, community centers, and other similar facilities with schools.

PSF-1.1.4 Promote smaller, neighborhood oriented school campuses to the extent possible to increase access for Estero students to attend school in proximity to home.

PSF-1.1.5 Protect the integrity of schools so that educational functions are not disrupted by the intrusion of incompatible land uses.

PSF-1.1.6 Participate in discussions with the Lee County School Board to establish a unique municipal based schooling initiative. This initiative will be the Village's mechanism to organize and implement volunteer and mentoring programs, internships, and enhancements to school facilities to ensure the schools in and around Estero are recognized as high quality schools and models for K-12 and lifetime learning centers.

SCHOOL FACILITY DEVELOPMENT

PSF-1.2

OBJECTIVE

On and off-site improvements associated with existing and new school facilities will be coordinated between the Village and the entity developing the facilities to ensure that schools are neighborhood oriented and well connected to other community facilities and residential areas with bicycle and pedestrian infrastructure.



Image by Johnson Engineering, Inc.

POLICIES

PSF-1.2.1 New developments adjacent to school properties will be required to provide a right of way and a direct safe access path for bicycle and pedestrian travel to existing and planned school sites and will connect to the neighborhood's existing and proposed bicycle and pedestrian improvements.

PSF-1.2.2 In order to ensure continuous pedestrian access to public schools, provisions for construction of facilities to address hazardous walking conditions pursuant to Florida Statute Section 1006.23, will be included in the Village's schedule of capital improvements adopted each fiscal year.

PSF-1.2.3 The Village staff shall evaluate school zones to consider safe crossing of children along major roadways and prioritize areas for sidewalk and bicycle facility improvements to increase the ability of children to walk or bicycle safely to school.

PSF-1.2.4 Coordinate existing and planned public school facilities with the plans for supporting infrastructure to assure safe access to schools, including safe bussing, sidewalks, bicycle paths, turn lanes, and signalization.

PSF-1.2.5 The Village and the School District will jointly determine the need for and timing of on-site and off-site improvements necessary to support new school facilities. The Village and the School District will explore opportunities for shared funding of necessary infrastructure improvements.

PSF-1.2.6 The Village may enter into an agreement with the School District identifying the timing, location, and the party or parties responsible for constructing, operating, and maintaining improvements necessary to support new school facilities.

PSF-1.2.7 Work with the School District staff toward maximizing the opportunity for an educational campus in conjunction with Estero Community Park, School District property and Estero High School.

LEVEL OF SERVICE (LOS)

PSF-1.3

OBJECTIVE

Uphold standards to maintain a public school system that offers a high quality educational environment, provides accessibility for all of its students, and ensures adequate school capacity to accommodate enrollment demand.



Image by Lee County School District

POLICIES

PSF-1.3.1 Establish and maintain LOS standards for public schools in order to ensure that there is adequate school capacity for all existing and expected High School, Middle School, Elementary School, and Special Purpose students. See the Lee County School District Facilities Map PSF-1 depicting the existing educational and public School District Facilities in Lee County. This Map also generally depicts the anticipated location of educational and ancillary plants over the five-year and long-term planning period

PSF-1.3.2 Adopt the following LOS standards for public schools, based upon Permanent Florida Inventory School Houses (FISH) capacity.

- A. Elementary Schools: 100% of Permanent FISH Capacity as adjusted by the School District annually to account for measurable programmatic changes.
- B. Middle Schools: 100% of Permanent FISH Capacity as adjusted by the School District annually to account for measurable programmatic changes.
- C. High Schools: 100% of Permanent FISH Capacity as adjusted by the School District annually to account for measurable programmatic changes.
- D. Special Purpose Facilities: 100% of Permanent FISH Capacity as adjusted by the School District annually to account for measurable programmatic changes.

For purposes of this subsection, a “measurable programmatic change” means a change to the operation of a school and measurable capacity impacts including, but not limited to, double sessions, floating teachers, year-round schools and special educational programs.

Relocatable classrooms may be utilized to maintain the LOS on a temporary basis when construction to increase capacity is planned and in process. The temporary capacity provided by relocatables may not exceed 20% of the Permanent FISH Capacity and may be used for a period not to exceed three years.

Relocatables may also be used to accommodate special education programs as required by law and to provide temporary classrooms while a portion of an existing school is under renovation.

PSF-1.3.3 Any modification of public school LOS standards must be accomplished by amending the School Concurrency Interlocal Agreement and the adoption of amendments to this comprehensive plan. No LOS will be amended without a showing that the amended LOS is financially feasible, supported by adequate data and analysis, and can be achieved and maintained within the period covered by the School District's Five Year Capital Facilities Plan.

PSF-1.3.4 Adopt the School District's School Choice Zone boundaries depicted on Map PSF-2, as Concurrency Service Areas (CSAs). CSAs exclude multizone magnet schools and Special Purpose Facilities. The Village is within the District's South Zone. Concurrency for new development will be measured against capacity in the South Zone. Special Purpose Facility capacity will be added to the total CSA capacity as these facilities potentially provide service to students from all CSAs. The Village staff will work with the School District as it evaluates expanding the number of CSAs and sub-zones to utilize the CSA Zone geography as the basis for measuring school concurrency.

PSF-1.3.5 The Village staff will work with the School District staff to discuss the need to amend the CSAs. School District staff will informally present any proposed modification to Village staff for initial comments and input. The School District will be the lead agency and will make application for an amendment to the Village Comprehensive Plan to change the CSAs.

PSF-1.3.6 Any proposed boundary changes to the CSAs require a demonstration by the School District that the change complies with the adopted LOS standard and that utilization of school capacity is maximized to the greatest extent possible.

PUBLIC SCHOOL CONCURRENCY MANAGEMENT SYSTEM

PSF-1.4

OBJECTIVE

Apply a public school concurrency management system consistent with Florida Statute Section 163.3180(6).

POLICIES

PSF-1.4.1 Implement school concurrency provisions through the Land Development Code.

PSF-1.4.2 The Village, with the assistance of the School District, will annually identify available school capacity as part of its concurrency management reporting. The report will identify total school capacity. Total school capacity includes existing capacity and the capacity created by school improvements programmed in the first three years of an adopted School District Capital Improvement Program. The School District will annually transmit to the County: a copy of the adopted School Capital Improvement Program; student enrollment by school type by CSA; and, capacity information by school type by CSA.

PSF-1.4.3 All proposed residential development activity (including all local development order requests such as applications for rezoning, comprehensive plan amendments and site plans) will be sent to the School Board for its school concurrency review. Such requests will be reviewed against the available total capacity by school type as identified in the annual concurrency report for the specific CSA in which the proposed development is located. If capacity is available or appropriate mitigation has been agreed to by the Village and the School District, a concurrency certificate may be issued, valid for three years. If capacity is not available in the CSA where the development is proposed, then the Village will examine if the contiguous CSAs have capacity. If capacity is not available in the CSA in which the proposed development is located or in a contiguous CSA and appropriate mitigation cannot be agreed to, no concurrency certificate will be issued. A concurrency certificate may be renewed for an additional 3-year period and may be extended according to provisions of the Land Development Code applicable to Development Orders).

PSF-1.4.4 The LDC will enumerate mitigation options for proposed developments that cannot meet school concurrency. Mitigation options may include, but are not limited to:

- A. The donation of land or funding of land acquisition or construction of a public school facility sufficient to offset the demand for public school facilities created by the proposed development; and
- B. Establishment of a Charter School with facilities constructed in accordance with the State Requirements for Educational Facilities (SREF) on a site that meets the minimum acreage provided in SREF and subject to guarantees that the facility will be conveyed to the School District at no cost to the District if the Charter School ceases to operate.

Proposed mitigation must be directed towards a permanent school capacity improvement identified in the School District's financially feasible work program, which satisfies the demands created by the proposed development. If mitigation can be agreed upon, the ~~County~~ Village and the School District must enter into an enforceable binding developer agreement with the developer. If mitigation cannot be agreed upon, the Village must deny the application based upon inadequate school capacity.

Relocatable classrooms will not be accepted as mitigation.

PSF-1.4.5 The following residential uses are exempt from the requirements of school concurrency:

- A. Single family lots having received final plat approval prior to the effective date of Lee County Ordinance No. 08-17.
- B. Multi-family residential development having received a final development order and concurrency certificate prior to the effective date of Lee County Ordinance No. 08-17 and said final development order and concurrency certificate are valid and active.

- C. Amendments to existing residential development approvals that do not increase the number of residential units or change the type of residential units proposed.
- D. Other residential uses that do not generate school age children such as licensed Adult Living Facilities or age restricted residential developments prohibiting persons under the age of 18 from residing there as permanent residents through recorded covenants and restrictions that cannot be amended for a period of 30 years.
- E. Developments of Regional Impact approved pursuant to Florida Statute Chapter 380 prior to July 1, 2005, but only as to the number of residential units authorized in the DRI Development Order.

PSF-1.4.6 For school concurrency purposes, the number of projected students from a proposed residential development will be calculated using the student generation rate for the unit type identified in the effective School Impact Fee Update Study. The projected number of students is the product of the number of residential units multiplied by the student generation rate for each unit type.

COORDINATION

PSF-1.5

OBJECTIVE

Village staff and School District staff will coordinate on procedures and information sharing so that all new public schools built within the Village will be consistent with the Comprehensive Plan and Future Land Use Map, will be co-located with other appropriate public facilities (when possible), and will have needed supporting infrastructure.

POLICIES

PSF-1.5.1 Adopt the School District Capital Improvement Program, and include it in the Village's annual Capital Improvement program update. The Village will prioritize projects that enhance the safety and connectivity between residential areas, other community facilities, and schools.

PSF-1.5.2 Level of service standards for public schools will be coordinated between the Village and The School District of Lee County through adoption of a School Concurrency Interlocal Agreement pursuant to Florida Statute Section 163.3180(6)(i).

PSF-1.5.3 Public educational facility site acquisition and site planning and permitting will be coordinated between the Village and The School District of Lee County through adoption of a Public Educational Facility Planning interlocal agreement consistent with Florida Statute Section 163.31777.

PSF-1.5.4 The School District staff will inform the Village staff prior to acquiring or disposing of land in the Village. The Village staff will provide technical information as available to school representatives and work cooperatively to assist in identifying suitable sites for new schools and opportunities to co-locate public facilities.

PSF-1.5.5 In order to maximize the benefits from joint planning efforts, the Village staff will coordinate with the School District to base respective plans on consistent projections of the amount, type, and distribution of population growth and student enrollment.

PSF-1.5.6 The Village staff will assist the School District in the development of siting criteria that promote smaller, neighborhood oriented school campuses to the extent possible to increase access for Estero students to attend school in proximity to home.

PSF-1.5.7 The Village staff will forward all applications for rezonings and comprehensive plan amendments that propose increases to school population to the School District for review. The Village will provide the School District with information as needed for the District to determine the impact of the proposed rezonings or amendments upon school capacity.

PSF-1.5.8 The Village staff will participate in County and School District efforts to identify issues relating to public school emergency preparedness, such as:

- A. The determination of evacuation zones, evacuation routes, and shelter locations.
- B. The design and use of public schools as emergency shelters.
- C. The designation of sites other than public schools as long-term shelters, to allow schools to resume normal operations following emergency events.

PSF-1.5.9 Coordinate with the School District on the potential for appointing a member to the Village's Local Planning Agency, the Planning and Zoning Board. Also coordinate on appointing liaisons between the School Board and Village Council to facilitate communication related to any growth and development of the Estero area and its schools.

PSF-1.5.10 Public educational facilities shall be considered for location within all future land use categories except Wetlands, Conservation, and Public Parks and Recreation.

PSF-1.5.11 Coordinate with the School District staff to evaluate the availability of sufficient land proximate to residential areas, and within the future land use categories in which public schools are allowed, to meet the projected need for public schools.

EDUCATION PROGRAMMING

PSF-1.6

OBJECTIVE

Encourage education providers to optimize the educational capacity of local schools and universities to inform school attendees, the general population, and newcomers to promote the Village as a desirable, clean, vibrant, healthy, and sustainability-minded community.

POLICIES

PSF-1.6.1 Support and promote programs for Pre-K through 12th grade school children, the general adult population, and newcomers to heighten awareness of our area's special characteristics.

PSF-1.6.2 Support a coordinated program that fosters the construction and implementation of projects, that enhance the schools and the community.

Lee County School District Summary of Revenue/Expenditures Available for New Construction and Remodeling Projects Only						
	2016 - 17	2017 - 18	2018 - 19	2019 - 20	2020 - 21	Five Year Total
Total Revenues	\$33,000,000	\$67,297,586	\$29,500,000	\$0	\$0	\$129,797,586
Total Project Costs	\$33,000,000	\$67,297,586	\$29,500,000	\$0	\$0	\$129,797,586
Difference (Remaining Funds)	\$0	\$0	\$0	\$0	\$0	\$0

Source: Lee County School District 2016 - 2017 Work Plan

Lee County School District Capacity Project Schedules									
Project Description			2016-17	2017-18	2018 - 19	2019 - 20	2020 - 21	Total	Funded
New High School in South Zone	Location Not Specified	Planned Cost	\$33,000,000	\$37,797,586	\$0	\$0	\$0	\$70,797,586	Yes
	Student Stations		0	2,103	0	0	0	2,103	
	Total Classrooms		0	78	0	0	0	78	
	Gross Sq Ft		0	375,407	0	0	0	375,407	
New High School in East Zone	Location Not Specified	Planned Cost	\$0	\$29,500,000	\$29,500,000	\$0	\$0	\$59,000,000	Yes
	Student Stations		0	0	2,103	0	0	2,103	
	Total Classrooms		0	0	78	0	0	78	
	Gross Sq Ft		0	0	375,407	0	0	375,407	
District Totals	Planned Cost		\$33,000,000	\$67,297,586	\$29,500,000	\$0	\$0	\$129,797,586	
	Student Stations		0	2,103	2,103	0	0	4,206	
	Total Classrooms		0	78	78	0	0	156	
	Gross Sq Ft		0	375,407	375,407	0	0	750,814	

Source: Lee County School District 2016 - 2017 Work Plan

PROCEDURES FOR MONITORING, EVALUATING, AND APPRAISING IMPLEMENTATION OF THE COMPREHENSIVE PLAN

ANNUAL UPDATE OF THE CAPITAL IMPROVEMENTS SCHEDULE

The Capital Improvements Element will be evaluated annually, and the 5-year Capital Improvements Schedule will be updated annually in accordance with Florida Statute 163.3177(3)(b). The annual review and update of the Capital Improvements Schedule and the Capital Improvements Element shall coincide with the Village's budget adoption process. Those policies in other elements of the comprehensive plan affecting capital improvements will also be reviewed.

SCHEDULED EVALUATION AND APPRAISAL OF THE COMPREHENSIVE PLAN

The Village of Estero will evaluate its Comprehensive Plan within 7 years of the adoption date of this plan, in accordance with Florida Statute 163.3191. If it is determined that comprehensive plan amendments are necessary, the Village shall prepare and transmit the proposed amendments within one year.

Prior to the required 7-year evaluation, the Village will also conduct a mid-cycle review within 3 years of the adoption date of this plan, at which time the Village will:

- A. Determine if the Plan objectives are being met;
- B. Determine if any of the proposed tasks have been completed;
- C. Identify any new tasks that should be undertaken; and
- D. If necessary, amend the Plan according to the findings of the mid-cycle review.

DEFINITIONS

ACTIVE RECREATION - Active recreation requires a certain degree of physical exertion. Recreational activities that are of this orientation are generally of an organized or team nature and usually require an established playfield or type of court to accommodate them (e.g. baseball, football, tennis, basketball).

AMORTIZATION – A method of eliminating nonconforming uses by requiring the termination of the nonconforming use after a specified period of time.

CAPITAL IMPROVEMENTS - The acquisition of land, real property, a physical facility, or the construction of a physical facility.

CHAMPION TREE – Trees designated through the State of Florida Forest Service Champion Tree Program.

CIVIC SPACE - Spaces, public or private, that, when located at street level are accessible to the general public including: plazas, sidewalk gathering spaces with seating, pocket parks and playgrounds that are scaled to the surrounding urban environment, and other functional public areas.

CLUSTERING - A development design technique that concentrates a group of buildings or uses in specific areas to allow the remaining area to be used for open space, buffering, joint-parking, recreation, water management, or protection of environmentally sensitive areas.

COASTAL HIGH HAZARD AREA (CHHA) - The area below the elevation of the category 1 storm surge line as delineated by the Village's CHHA Map.

COMPATIBILITY – A condition in which land uses or conditions can coexist in relative proximity to each other in a stable fashion over time such that no use or condition is unduly negatively impacted directly or indirectly by another use or condition.

COMPLEMENTARY USES – Uses of land generally within a walkable distance from one another, which are compatible with each other and would serve the same users without requiring a car or a bike/walk trip on a circuitous or inhospitable route.

COMPLETE STREETS – Transportation facilities designed to serve the transportation needs of transportation system users of all ages and abilities, including but not limited to cyclists, freight handlers, motorists, pedestrians, and transit riders.

CONSERVATION EASEMENT - A right or interest in real property that is appropriate to retaining the land or water areas predominately in the natural, scenic, open, agricultural, or wooded condition. See Florida Statute Section 704.06.

DENSITY - The number of residential dwelling or housing units per gross acre (du/acre).

For purposes of the Village Center Land Use Category, densities specified are gross residential densities. For the purpose of calculating gross residential density, the total acreage of a development includes those lands to be used for residential uses, and includes land within the development proposed to be used for streets and street rights of way, utility rights-of-way, public and private parks, recreation and open space, schools, community centers, and facilities such as police, fire and emergency services, sewage and water, drainage, and existing man-made waterbodies contained within the residential development. For land in Tiers 2, 3, and 4 non-residential land is included in density calculations.

DEVELOPMENT - Has the meaning given in Florida Statute Chapter 380.

DEVELOPMENT ORDER - An order granting, denying, or granting with conditions an application for a development permit.

DEVELOPMENT PERMIT - Includes any building permit, zoning permit, subdivision approval, rezoning, certification, special exception, variance, or any other official action of the Village having the effect of permitting the development of land.

DWELLING UNIT (DU) – A single unit providing complete, independent living facilities for one or more persons, including permanent provisions for living, sleeping, eating, cooking and sanitation.

EQUIVALENT RESIDENTIAL CONNECTION (ERC) - The total number of meter equivalents using the methodology of the Florida Public Service Commission. This term is synonymous with "Equivalent Residential Units" as used by the Florida Public Service Commission. It is used to convert commercial and industrial water or sanitary sewer use into standard units, based on typical use in dwelling units.

FACADE - The elevations of a building usually set parallel to the frontage line. Facades define the public space and are subject to requirements additional to those of elevations such as architectural standards, assigned frontage types and height restrictions.

FEASIBILITY - The terms "feasible" or "where feasible" in reference to a goal, objective or policy in this Comprehensive Plan means that the ability to perform a referenced action will be evaluated and regulated by weighing and balancing the public benefits of such an action in terms of quality of life, sustainability, attractive, high quality design, and preserving and protecting the environment of the Village, against economic factors such as the additional cost of such action or regulation or other market factors which may mitigate against taking such an action. Such evaluation or balancing test is not simply a measure of the underlying ability to take an action at whatever price, nor is it a requirement that any additional cost would render an action not feasible.

When applied to a decision interpreting a comprehensive plan goal, objective or policy, the determination of feasibility shall be made by the Village Council pursuant to its legislative discretion.

FLOODING - A general or temporary condition of partial or complete inundation of normally dry land areas from: (1) overflow of inland or tidal water; or (2) unusual and rapid accumulation or runoff of surface water from any source.

FLOOD PRONE AREA OR FLOODPLAIN - Any land area susceptible to being inundated by water from any source.

FLOOR AREA RATIO (FAR) - A measure of intensity expressing the maximum allowable floor area permitted on a lot. The FAR is equivalent to the total floor area of all commercial buildings divided by the total area of the lot, expressed in square feet. The FAR represents the relationship of the developed square footage of the lot to the square footage of the site.

FLORIDA FRIENDLY LANDSCAPING - Quality landscapes that conserve water, protect the environment, are adaptable to local conditions, and are drought tolerant. The principles of such landscaping include planting the right plant in the right place, efficient watering, appropriate fertilization, mulching, attraction of wildlife, responsible management of yard pests, recycling yard waste, reduction of stormwater runoff, and waterfront protection. Additional components include practices such as landscape planning and design, soil analysis, the appropriate use of solid waste compost, minimizing the use of irrigation, and proper maintenance

FLOWWAY - A defined area that conveys surface water during typical seasonal weather patterns. The flow-way may contain uplands, wetlands, defined natural or artificial channels, or a combination thereof. A flow-way may be natural or man-made.

A natural flow-way is an area of lower topographic relief where surface water moves within variable dimensions or a well-defined channel. The area of flow in this case has enough general confinement to exhibit surface water flow characteristics and is determined through reasonable scientific judgment utilizing all available information including without limitation a review of all the following: vegetation, hydrology, soils maps, aerial photography, topographic maps, USGS maps, drift lines, rack lines, sediment deposits, soils and root scour, absence of litter or groundcover, and field verifications. A natural flow-way can be a series of lower elevation upland areas that allow otherwise isolated wetlands to interconnect when surface water levels rise high enough during typical high water seasonal level to form a continuous flow path. Natural flow-ways typically include but are not limited to rivers, creeks, streams, sloughs, interconnected wetlands, and associated flood plain.

A man-made naturalized flow-way is a constructed surface water management system consisting of soft features used for nutrient uptake, surface water treatment, and/or surface water conveyance. Man-made naturalized flow-ways typically include but are not limited to filter

marshes, created wetlands, swales planted with native vegetation, created streams/creeks, created pond or lake systems interconnected through native vegetation areas, or combinations thereof.

A man-made structural flow-way is a physical connection between surface water management basins. Man-made structural flow-ways typically include but are not limited to culverts, ditches, canals, pipelines, mowed grass swales, rip-rap swales, or combinations thereof. This will not include roadways, fire breaks, or similar man-made structures.

FUNCTIONAL STREET CLASSIFICATION - Functional classification is the process by which streets and highways are grouped into classes, or systems, according to the character of service they are intended to provide.

GREEN INFRASTRUCTURE - Surface water management systems that are "soft" features such as preserved/restored flow-ways, created flow-ways, lakes with littoral plantings, swales planted with native grasses, filtration marshes, preserved/restored wetlands, created wetlands, or other similar design features.

GREENWAY - A trail that is outside a vehicular right-of-way, usually in a natural setting.

HERITAGE TREE – Any native species with at least a 20 inch caliper diameter at breast height.

HISTORIC RESOURCE - Any prehistoric or historic district, site, building, object, or other real or personal property of historical, architectural, or archaeological value. These properties or resources may include, but are not limited to trees, monuments, memorials, Indian habitations, ceremonial sites, abandoned settlements, sunken or abandoned ships, engineering works, treasure trove, artifacts, or other objects with intrinsic historical or archaeological value, or any part thereof, relating to the history, government, and culture of the Village, Lee County, the state of Florida, or the United States (s.267.021 (3), F.S. 1986).

HISTORIC TREE – (see definition of historic resource).

HUMAN-SCALE - The use of buildings with details, elements and materials that are inviting to pedestrians and site design elements clearly oriented to human activity, such as limited block length, limited distance between entrances and reduced blank or dead façade space.

INCREASE IN INTENSITY - A significant or substantial increase in the intensity of the land use for a particular parcel which could reasonably be determined to adversely impact neighboring or surrounding land uses or is otherwise detrimental to public health, safety or welfare. Shifts of types of uses, or conversion of uses from one type of use to another. within the same planned development, which do not increase overall intensities of the development may be deemed an Increase in Intensity if such shifts or conversions adversely impact surrounding land uses, either within the overall planned development or external thereto, and which adverse impacts were not reviewed as a part of the existing zoning approval. For purposes of determining vested rights in the Village Center Future Land Use category, an increase in the intensity of a parcel by ten percent

(10%) or more in size, height, lot coverage, floor area ratio, or traffic generation with respect to a development parcel shall be deemed to be an Increase in Intensity.

INFRASTRUCTURE - Facilities needed to sustain all land use activities. Comprehensive Plan related infrastructure includes, potable water, sewer, water management, utilities, and transportation facilities.

INTENSITY - The extent to which land is being used. It is an indication of the amount and degree of development in an area and a reflection of the effects generated by that use. Without limiting the generality of the foregoing, intensity is usually determined in relationship to customarily non-residential uses based on a measurement of the use, size, impact on natural resources or facilities and services, height, lot coverage, traffic generation, and floor area ratios.

LEVEL OF SERVICE (LOS) - An indicator of the extent or degree of service provided by, or proposed to be provided by a facility based on and related to the operational characteristics of a facility. Levels of service are generally expressed as the capacity per unit of demand for each public facility.

LINER BUILDING - Type of building constructed in front of a parking lot, cinema, supermarket etc., to conceal large expanses of blank space or wall and to face the street space with a façade that has doors and windows opening onto the sidewalk.

LOW INCOME - A person or household whose annual (gross) income does not exceed the 80% percent of the area median income, as determined by HUD.

MARINA - A commercial or industrial Water-Dependent Use located on property adjacent to water with direct access to a navigable channel. The primary function must be to provide commercial dockage, mooring, storage and service facilities for watercraft and land-based facilities and activities necessary to support the water dependent use. The term "marina" does not apply to docks, davits, boathouses and similar docking facilities that are accessory or ancillary and subordinate to:

1. residential buildings that are located on the same premises and under the same ownership or control as the docks, davits, boathouses, boat ramps, and similar docking facilities; and
2. commercial or industrial establishments that are not Water-Dependent uses.

For purposes of this definition only, "residential building" means a mobile home, single-family, two-family, duplex, townhouse or multiple-family dwelling.

MIXED USE - Complementary uses of land generally within a walkable distance from one another. Complementary means uses are compatible with each other and would serve the same users without requiring a car or a bike/walk trip on a circuitous or inhospitable route. Complementary uses are the opposite of conflicting uses, for example industrial uses or commercial uses that have features near residential uses which could negatively impact the quality of life or interfere with the quiet enjoyment of such residential uses. These uses may be combined within the same Mixed-

Use Building or may be grouped together in cohesive neighboring buildings with limited separation, unified form and strong pedestrian interconnections to create a seamless appearance.

MIXED USE BUILDING – Mixed-Use Building means a building that contains at least two different complementary land uses (i.e. commercial and residential, R&D and residential, office and residential, commercial and civic use open to the public).

MIXED USE PATTERN - A physical pattern where streets form an interconnected network within and surrounding various parcels in an overall development area, neighborhood, or group of interconnected neighborhoods. A variety of methods to subdivide such areas and neighborhoods may be utilized to develop individual buildings that can accommodate a variety of complementary land uses and building types.

MODERATE INCOME - A person or household whose annual (gross) income does not exceed the 120% percent of the area median income, as determined by HUD.

OPEN SPACE - Land, public or private, which may be either unoccupied or predominately unoccupied by buildings or structures, having use for parks, recreation, water management, vegetation, agriculture, conservation, protection or preservation of water resources, historic or scenic resources, green space, green belts, natural rivers and streams, forests, wetlands, beaches and dunes, wildlife habitat, preserves, sanctuaries, reserves and refuges, and air and water.

PASSIVE RECREATION - Passive recreation activities are ones which require a minimal amount of physical effort but help in promoting physical and psychological refreshment. Such activities include: picnicking, camping, strolling in the park, and water-oriented recreation in the forms of fishing and boating.

PLACEMAKING - Effective amenitizing, programming and design of a mix of uses within a pedestrian environment to create community scale in a distinctive setting that accommodates social interaction.

PLANNED DEVELOPMENT - A development that is designed and developed as a cohesive, integrated unit under single ownership or unified control which permits flexibility in building siting, mixture of housing types or land uses, clustering, common functional open space, the sharing of services, facilities, and utilities, and protection or enhancement of environmental and natural resources. A "planned development" may be for residential, commercial, industrial, or other specific purposes, or a combination thereof.

PUBLIC RECREATION FACILITIES - Land and appurtenant facilities that are provided by a governmental agency, private developer or charitable ~~conservation~~ organization for recreational use by the general public.

QUASI-PUBLIC USES – ~~f~~Facilities that serve the public but are not necessarily owned or operated by a government entity, including community centers, meeting halls, recreation centers, clubhouses, libraries, museums and galleries, art display areas (inside or outside), performing arts

auditoriums and facilities (including amphitheaters), passive and active recreation areas, trails, community gathering spaces, plazas, daycare, and other uses that are similar in nature.

REDEVELOPMENT - Development activity characterized by replacement of existing dilapidated or underperforming structures. The new development is usually at a higher level of intensity or density.

REHABILITATION - The act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use.

RESEARCH AND DEVELOPMENT - Establishments primarily engaged in commercial physical and biological research and development, and noncommercial research establishments; performing commercial business, marketing, opinion, and other economic, sociological, and educational research; performing noncommercial research into and dissemination of, information for public health, education, or general welfare; and establishments primarily engaged in providing testing services.

RESOURCE BASED PARKS - These are areas that possess a high degree of natural beauty through the land, air, and water resources that compose them. These areas generally have a higher level of intrinsic value, i.e., visual and other aesthetic qualities of their natural environment. Examples of resource-based recreation areas are beaches, wetland preserves, and riverfront parks.

RESTORATION - The act of accurately recovering the form and details of a property and its setting as it appeared at a particular period of time by means of the removal of later work or the replacement of missing earlier work.

REUSE WATER - Treated wastewater which, as a commodity, is can be sold to various users for residential, commercial or agricultural irrigation, grounds management (institutional lawns, golf courses), or industrial use.

SANITARY SEWER SYSTEM - The infrastructure required to collect, transport, treat, and dispose of water-borne wastes, and the contaminated water that carries such wastes, from many individual and varied land uses. This definition of "sanitary sewer system" includes those systems operated by developers and homeowner or condominium associations, as well as by county or municipal governments or other public agencies, and by franchised or certificated utility companies. This does not include septic tank systems.

STREETSCAPE - The layer between the lot line or building facade and the edge of the vehicular lanes. Principal variables are type and dimension of curbs, walks, planters, street trees, and streetlights.

TRANSIT-ORIENTED DEVELOPMENT (TOD) - Development located within walking distance of a transit stop using a mixed use development pattern and Traditional Neighborhood Development standards.

URBAN SERVICES - The requisite services, facilities, capital improvements, and infrastructure necessary to support growth and development at levels of urban density and intensity. Urban services as used in this plan include, but are not limited to:

- public sewer and water
- paved streets and roads
- public transit
- parks and recreation facilities
- urban levels of police, fire, and emergency services
- urban surface water management
- schools
- employment, industrial, and commercial centers
- institutional, public, or administrative facilities
- community facilities such as senior citizens' centers, libraries, and community centers

VERY LOW INCOME - A person or household whose annual (gross) income does not exceed the 50% percent of the area median income, as determined by HUD.

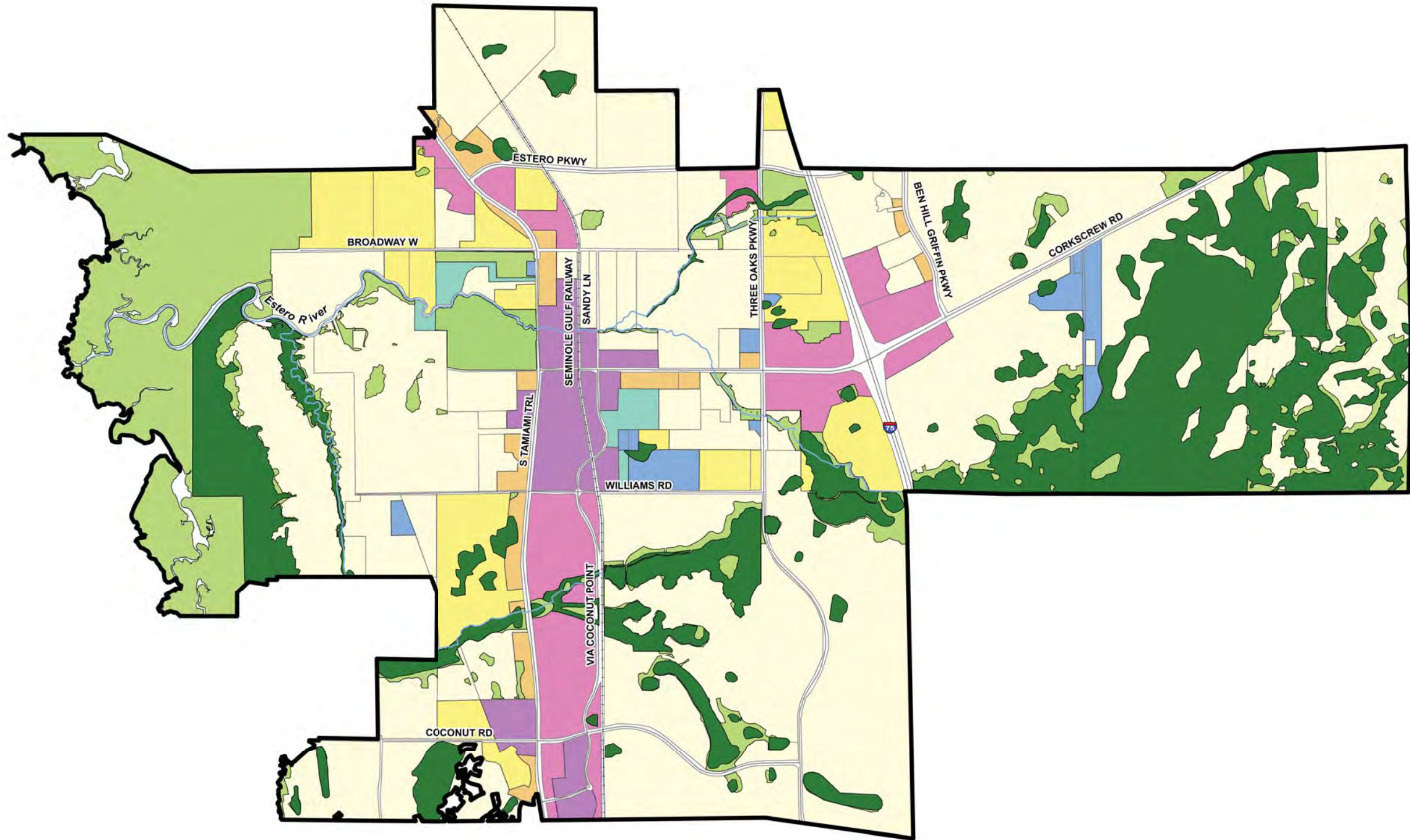
WALKABLE – A characteristic of mixed-use neighborhoods or districts where people can safely walk to nearby destinations on sidewalks or paths that encourage comfortable movement without driving. Nearby destinations are generally viewed as those that can be reached by a 5-minute walk or about ¼ mile in distance.

WATER-DEPENDENT USES - Land uses for which water access is essential and which could not exist without water access.

WETLANDS - Areas that are inundated or saturated by surface water or ground water at a frequency and a duration sufficient to support, and under normal circumstances do support, a prevalence of vegetation typically adapted for life in saturated soil. Wetland boundaries will be determined by using the methodology ~~in Florida Administrative Code Chapter 17-340 as ratified and amended~~ in Florida Statute Section 373.4211.

~~**XERISCAPE** – A style of landscaping using drought-tolerant (emphasizing native) plant materials and water conserving design and maintenance techniques in order to reduce the demand for irrigation water in man-made environments.~~

MAP SERIES



The Future Land Use Map shows the approximate boundaries of wetlands. The exact boundaries will be determined in accordance with the administrative process outlined in Policy FLU-1.8.2.

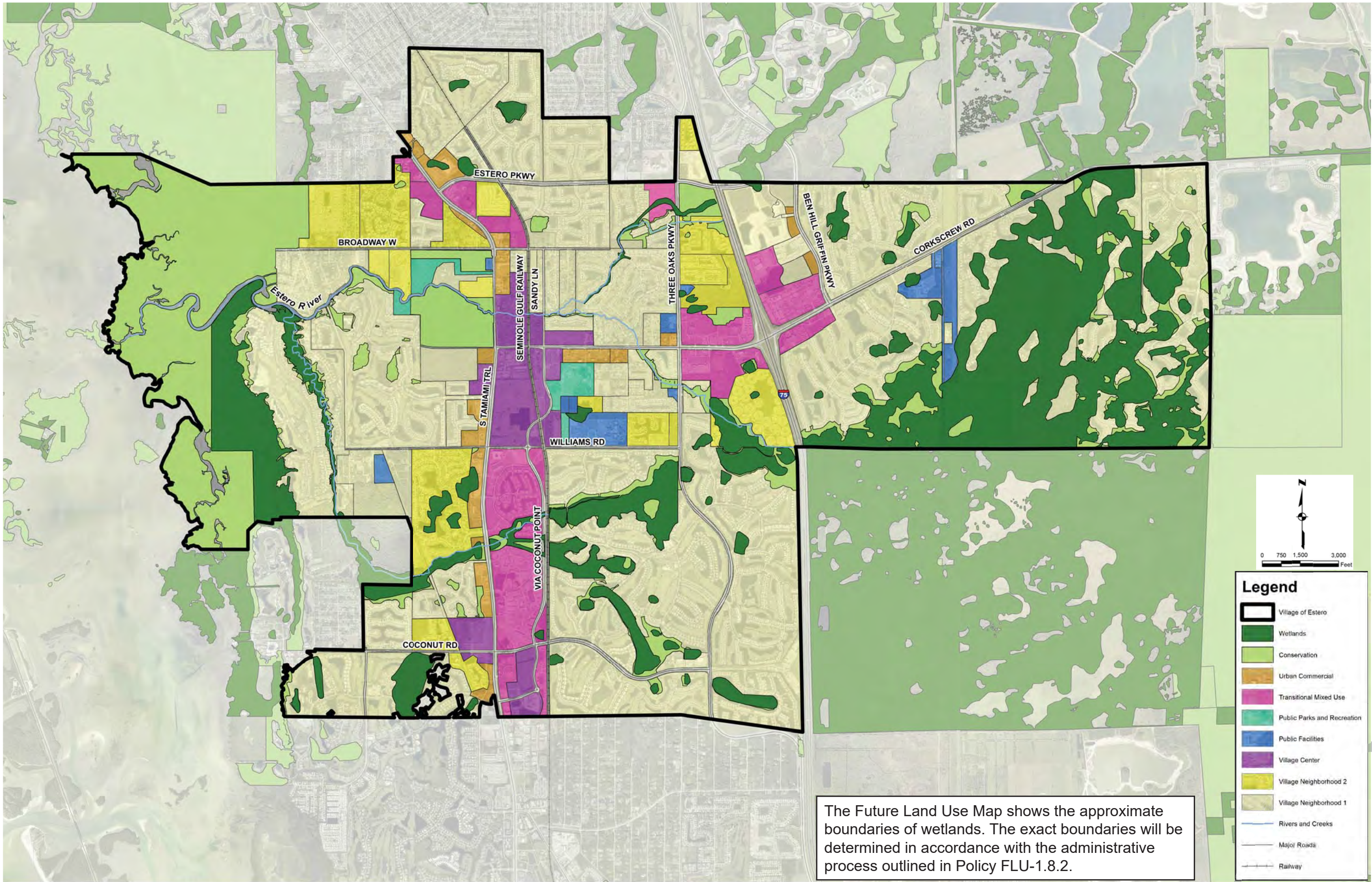
REVISIONS

1 Addition of wetland note and Trailside VN-2 designation.
Title change to include 2040.

May 2018

NOTES

1. Future Land Use areas based on GIS Data from Lee County and Lee County Property Appraisers offices.
2. The Village of Estero boundary is from Lee County government dated October 1, 2015.



REVISIONS

1. Addition of wetland note and Trailside VN-2 designation.
Title change to include 2040.

May 2018

NOTES

1. Future Land Use areas based on GIS Data from Lee County and Lee County Property Appraisers offices.
2. Aerial shown is from Lee County dated January 2016.
3. The Village of Estero boundary is from Lee County government dated October 1, 2015.



COASTAL HIGH HAZARD AREA (CHHA)

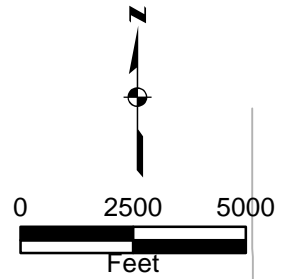
DATE
JANUARY 2018

PROJECT
20160267-000


FILE NO.


SCALE
AS SHOWN


MAP
FLU-2





Legend

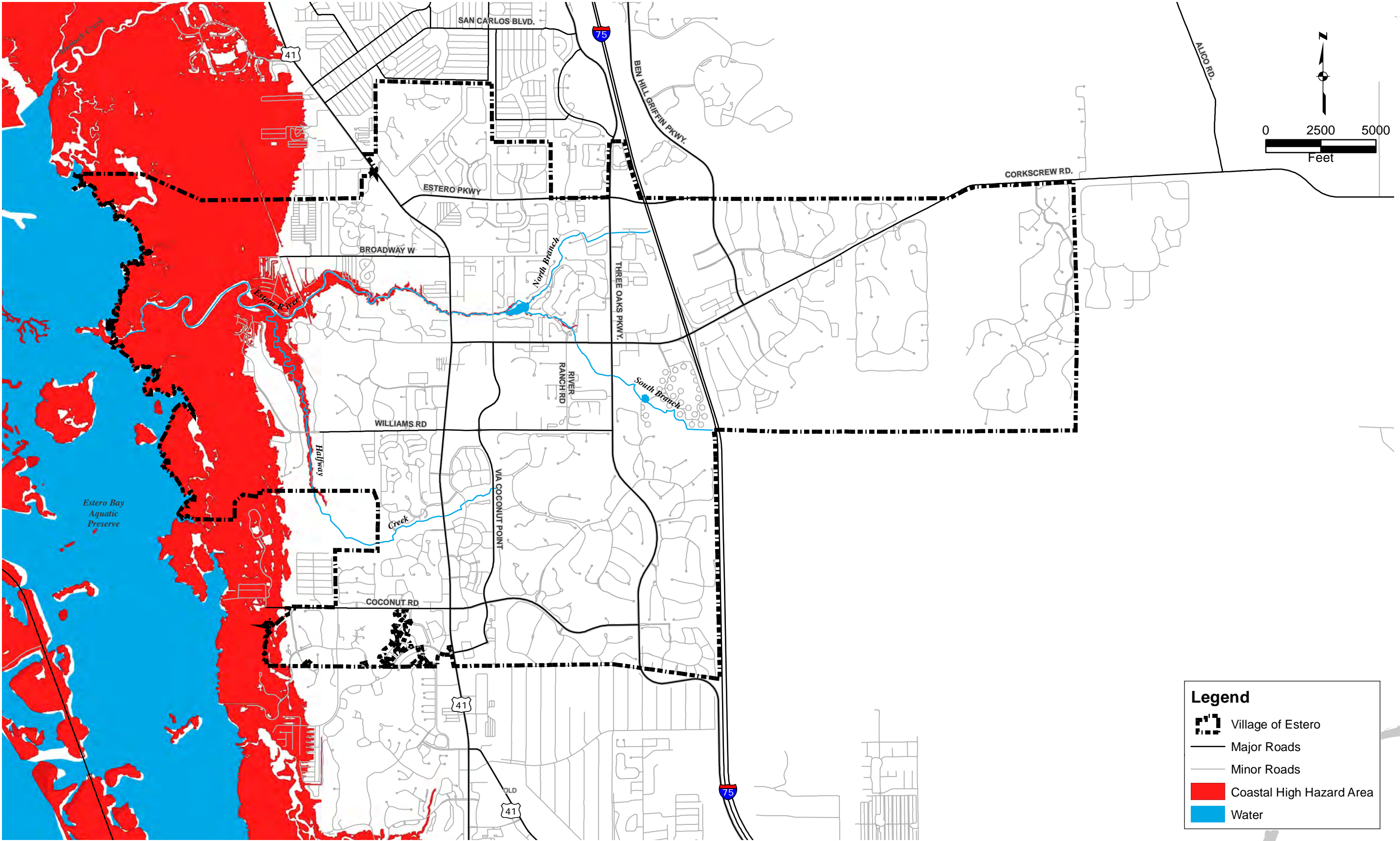
 Village of Estero

 Major Roads

 Minor Roads

 Coastal High Hazard Area

 Water



REVISIONS

- | NO. | DESCRIPTION | DATE |
|-----|-----------------|------------|
| 1. | Color Revisions | April 2018 |

NOTES

- Coastal High Hazard Area is from Lee County Lee Plan Map 5 generated December 2016.
- The Village of Estero boundary is from Lee County government dated October 1, 2015.





SOILS MAP

DATE
OCTOBER 2016

PROJECT
20160267-000

FILE NO.

SCALE
AS SHOWN

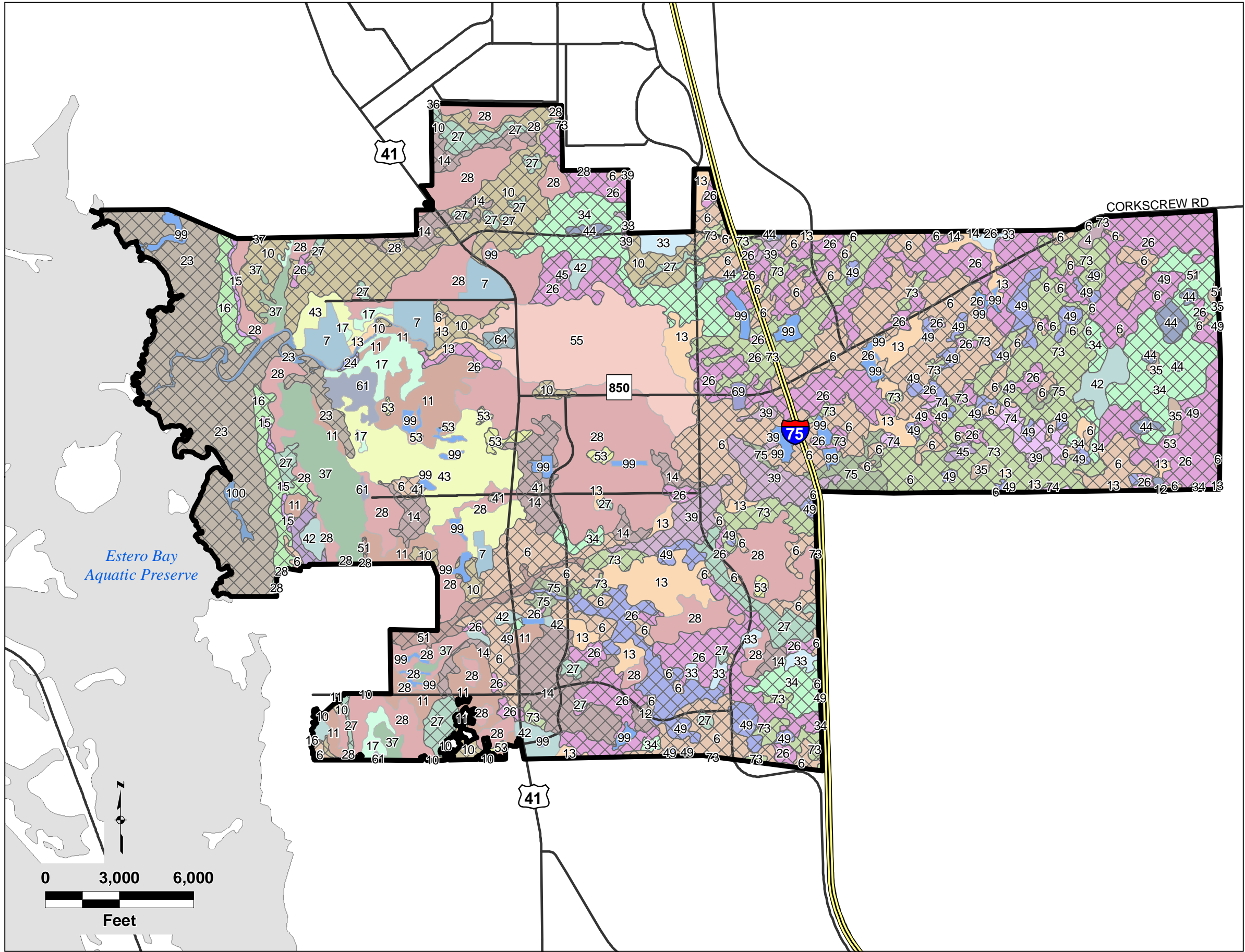
MAP
FLU-3

LEGEND



Map ID	Description	Hydric Status
4	CANAVERAL-URBAN LAND COMPLEX	N
6	HALLANDALE FINE SAND	N
7	MATLACHA-URBAN LAND COMPLEX	N
10	POMPANO FINE SAND	Y
11	MYAKKA FINE SAND	N
12	FELDA FINE SAND	Y
13	BOCA FINE SAND	N
14	VALKARIA FINE SAND	Y
15	ESTERO MUCK	Y
16	PECKISH MUCKY FINE SAND	Y
17	DAYTONA SAND	N
23	WULFERT MUCK	Y
24	KESSON FINE SAND	Y
26	PINEDA FINE SAND	Y
27	POMPANO FINE SAND, DEPRESSIONAL	Y
28	IMMOKALEE SAND	N
33	OLDSMAR SAND	N
34	MALABAR FINE SAND	Y
35	WABASSO SAND	Y
36	IMMOKALEE-URBAN LAND COMPLEX	N
37	SATELLITE FINE SAND	N
39	ISLES FINE SAND, DEPRESSIONAL	Y
41	VALKARIA FINE SAND, DEPRESSIONAL	Y
42	WABASSO SAND, LIMESTONE SUBSTRATUM	N
43	SMYRNA FINE SAND	Y
44	MALABAR FINE SAND, DEPRESSIONAL	Y
45	COPELAND SANDY LOAM, DEPRESSIONAL	Y
49	FELDA FINE SAND, DEPRESSIONAL	Y
51	FLORIDANA SAND, DEPRESSIONAL	Y
53	MYAKKA FINE SAND, DEPRESSIONAL	Y
55	COCOA FINE SAND	N
61	ORSINO FINE SAND	N
64	HALLANDALE-URBAN LAND COMPLEX	N
69	MATLACHA GRAVELLY FINE SAND	N
73	PINEDA FINE SAND, DEPRESSIONAL	Y
74	BOCA FINE SAND, SLOUGH	Y
75	HALLANDALE FINE SAND, SLOUGH	Y
99	Water	N/A
100	Waters of the Gulf of Mexico	N/A

\\Firms01\Drawings\2016\20120267-000\arcgis\soils table.xls



REVISIONS

NOTES

1. The soils layer shown is from USDA / NRCS through Florida Geographic Data Library.
2. This soils layer is commonly known as STATSGO and was published in 1994.
3. The Village of Estero boundary is from Lee County government dated October 1, 2015.

LaRue
planning

JOHNSON
ENGINEERING



FLOODPLAINS

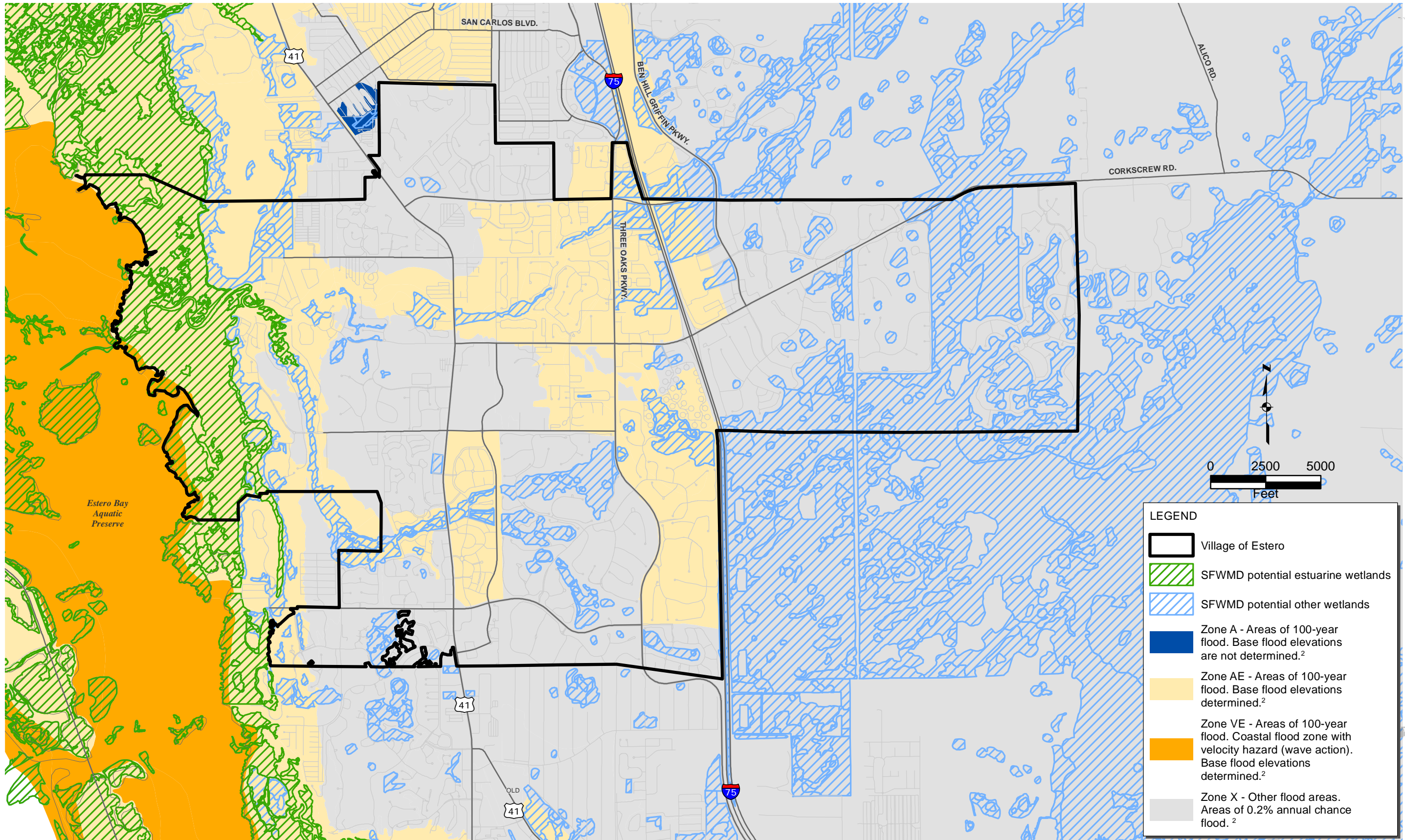
DATE
MAY 2018

PROJECT
20160267-000

FILE NO.

SCALE
AS SHOWN

MAP
FLU-4



REVISIONS

NOTES

1. The flood zones shown were downloaded from the FEMA website on January 4, 2016.
2. See FEMA panels for complete flood zone descriptions.
3. The SFWMD potential wetlands layer shown displays most of the 6000 series FLUCFCS codes from the SFWMD version 2008-2009 land use / land cover data set. 6000 series FLUCFCS codes not displayed in this layer are displayed in the SFWMD potential estuarine wetlands layer.
4. The SFWMD potential estuarine wetlands layer shown displays FLUCFCS codes 6120, 6420, 6510 and 6520. This layer is based on the SFWMD version 2008-2009 land use / land cover data set.
5. The Village of Estero boundary is from Lee County government dated October 1, 2015.



FUNCTIONAL CLASSIFICATION MAP

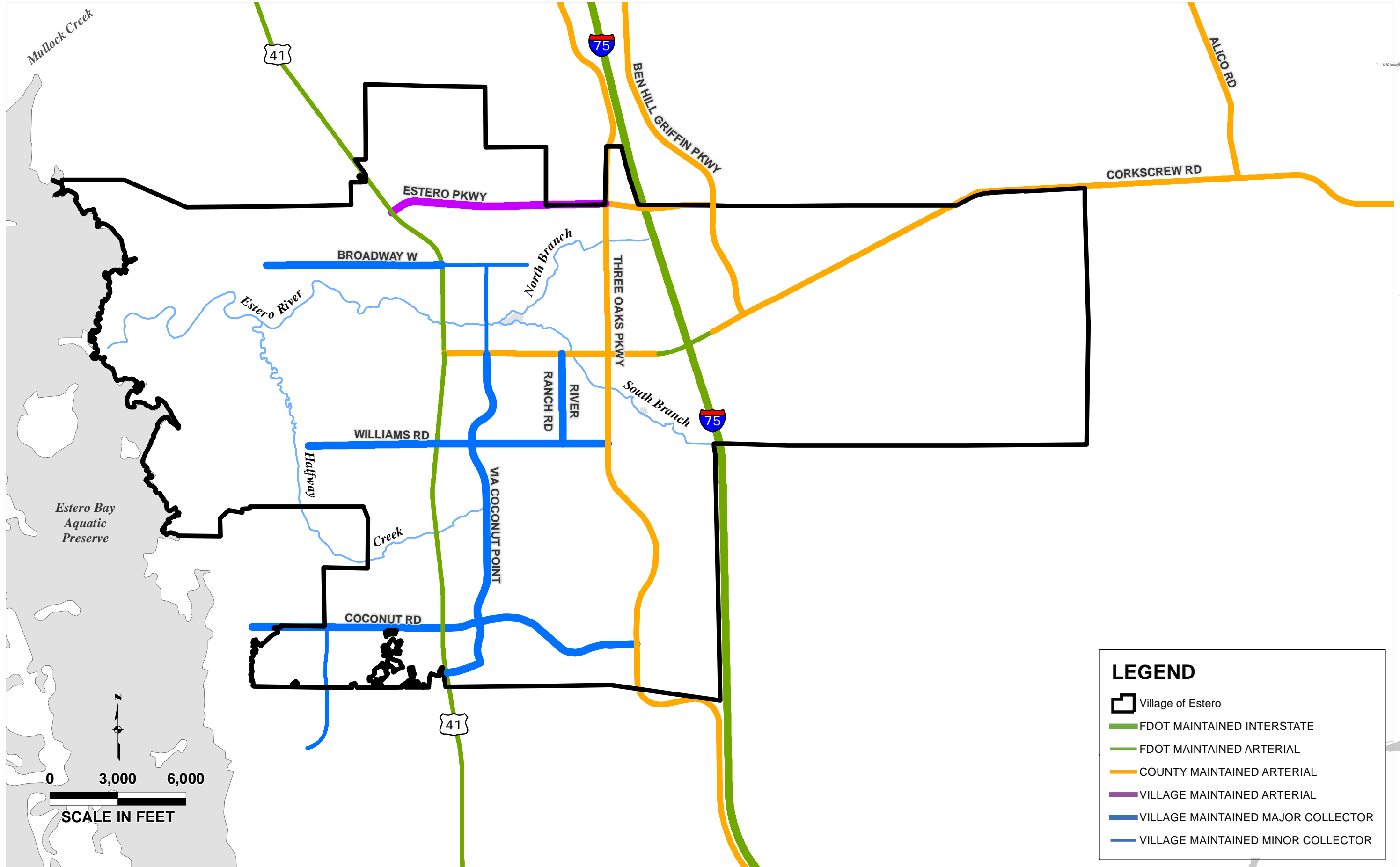
DATE
NOVEMBER 2017

PROJECT
20160267-000

FILE NO.

SCALE
AS SHOWN

MAP
TR-1



LEGEND

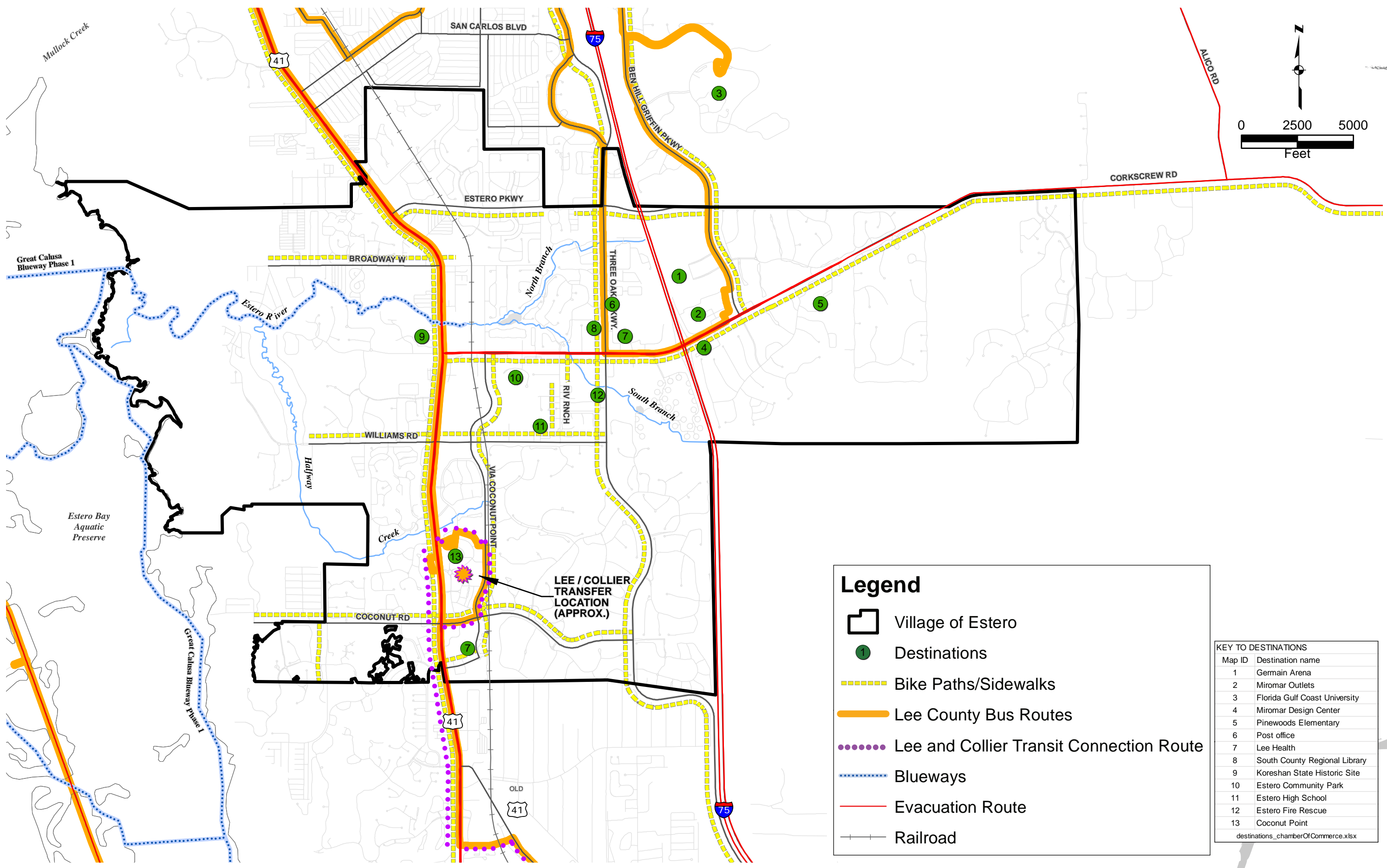
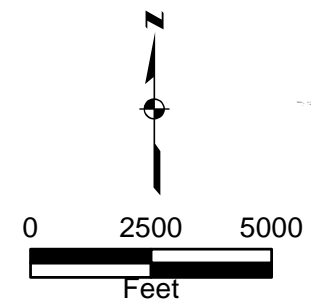
- Village of Estero
- FDOT MAINTAINED INTERSTATE
- FDOT MAINTAINED ARTERIAL
- COUNTY MAINTAINED ARTERIAL
- VILLAGE MAINTAINED ARTERIAL
- VILLAGE MAINTAINED MAJOR COLLECTOR
- VILLAGE MAINTAINED MINOR COLLECTOR

NOTES

1. Roadway classifications based on Lee County DOT mapping and GIS data dated October 2015.
2. The Village of Estero boundary is from Lee County government dated October 1, 2015.
3. US 41 and I-75 are maintained by FDOT. Corkscrew Road and Three Oaks Pkwy are maintained by Lee County. All other non-private roads in the Village of Estero are maintained by the Village of Estero.

REVISIONS

- | | | |
|---|---------------------|----------|
| 1 | Layer color change. | May 2018 |
|---|---------------------|----------|



Legend

- Village of Estero
- Destinations
- Bike Paths/Sidewalks
- Lee County Bus Routes
- Lee and Collier Transit Connection Route
- Blueways
- Evacuation Route
- Railroad

KEY TO DESTINATIONS	
Map ID	Destination name
1	Germain Arena
2	Miromar Outlets
3	Florida Gulf Coast University
4	Miromar Design Center
5	Pinewoods Elementary
6	Post office
7	Lee Health
8	South County Regional Library
9	Koreshan State Historic Site
10	Estero Community Park
11	Estero High School
12	Estero Fire Rescue
13	Coconut Point
destinations_chamberOfCommerce.xlsx	

REVISIONS	
1.	Title change, optional land trail opportunity corridor removal, rail and evacuation routes addition.
May 2018	

- NOTES**
1. The Lee County bus routes shown are from Lee County government dated August of 2016.
 2. The Lee and Collier transit connection route is from Collier County government.
 3. The Blueways layer is from Lee County government dated 2006.
 4. The bike paths/sidewalks shown are per a Chamber of Commerce map, Estero Infrastructure Inventory (2016) and aerial interpretation.
 5. The destinations shown were taken from a Chamber of Commerce map.
 6. Railroad linework traced from Lee County 2015 aerial imagery.
 7. Evacuation Routes are established by Southwest Florida Regional Planning Council. Linework is from Lee County GIS Open Data.
 8. The Village of Estero boundary is from Lee County government dated October 1, 2015.
 9. Bike paths/sidewalks depicted on this map represent locations where bike paths or sidewalks are on one side of the road or both sides of the road.

TRANSPORTATION FACILITIES 2023

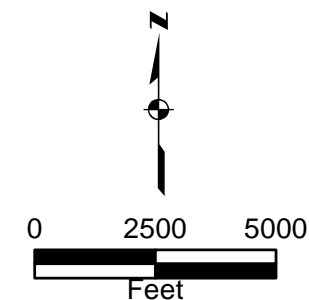
DATE
MAY 2018

PROJECT
20160267-000

FILE NO.

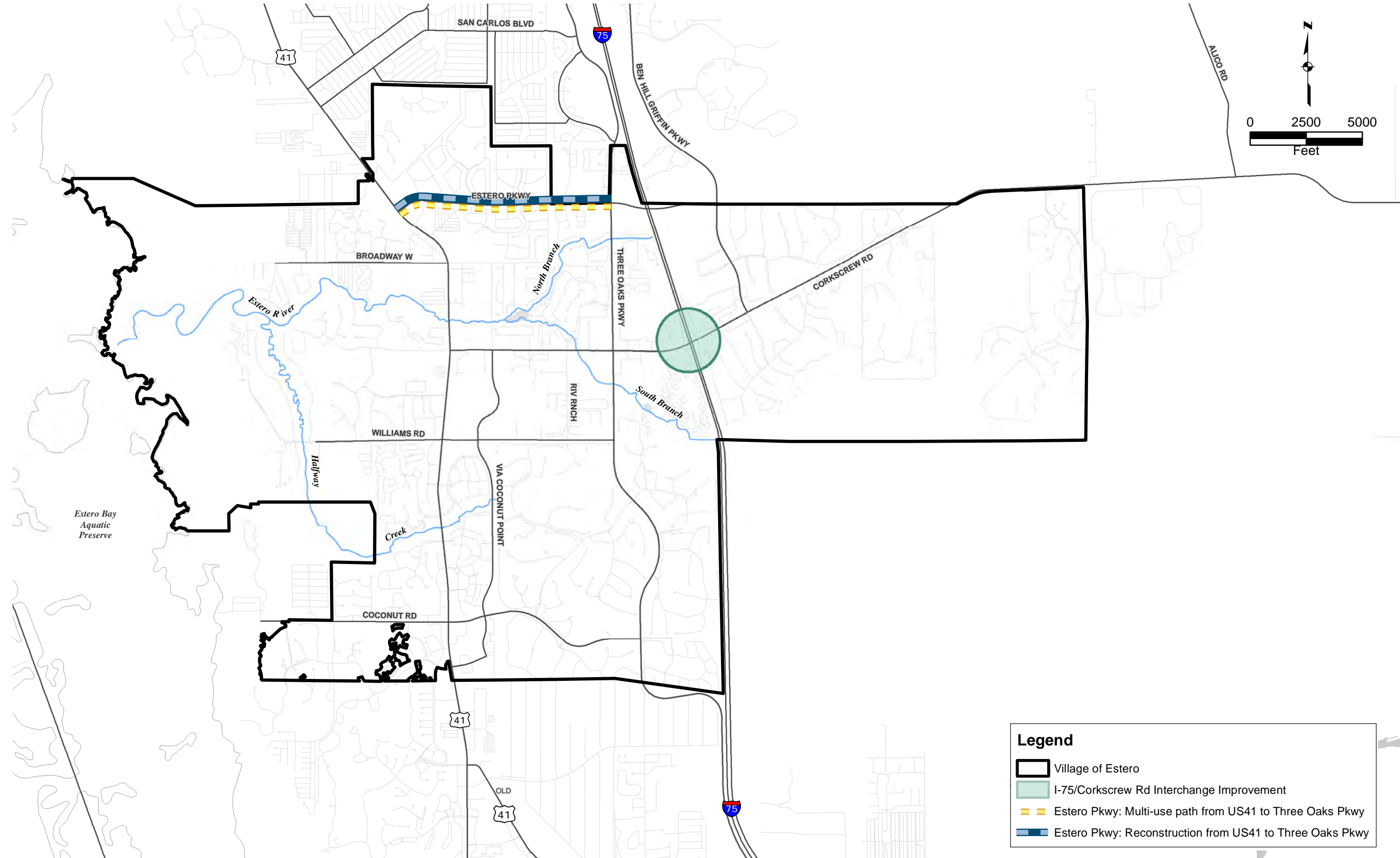
SCALE
AS SHOWN

MAP
TR-3



Legend

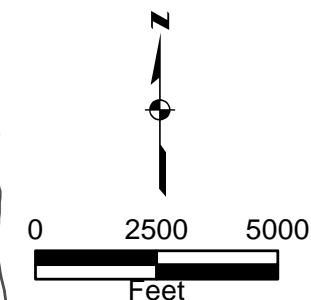
- Village of Estero
- I-75/Corkscrew Rd Interchange Improvement
- Estero Pkwy: Multi-use path from US41 to Three Oaks Pkwy
- Estero Pkwy: Reconstruction from US41 to Three Oaks Pkwy



REVISIONS

NOTES

1. I-75/Corkscrew Rd Interchange Improvement source: Lee County Metropolitan Planning Organization (MPO) 2040 Transportation Plan.
2. Estero Pkwy improvements source: Village of Estero Public Works.
3. Improvements shown are cost feasible.
4. The Village of Estero boundary is from Lee County government dated October 1, 2015.



Legend

Village of Estero

Optional Land Trail Opportunity Corridors

Bike/Ped/Multi-Use Trail Projects

US41: Multi-use path from Lychee Ln to Corkscrew Rd

Fixed-Route Bus Improvements

Existing Route 60: Increased service frequency and hours of service

Existing Route 140: Increased service frequency and hours of service

Existing Route 240: Increased service frequency and hours of service

New Local Service Route: Estero Circulator

New Local Service Route: Treeline/Ben Hill Griffin

New Premium Service Route: Lee Collier LinC (Route 600)

Road/Highway Projects

Corkscrew Rd: Lane Widening (4 to 6) from US41 to Ben Hill Griffin Pkwy

Corkscrew Rd. Lane Widening (2 to 4) from Ben Hill Griffin Pkwy to Alico

Williams Rd: Lane Widening (2 to 4) from US41 to Three Oaks Pkwy

Estero Pkwy: Extension from Ben Hill Griffin Pkwy to Corkscrew Rd

REVISIONS

NOTES

1. The optional land trail opportunity corridors shown include Florida Department of Environmental Protection corridors dated December 2015 and Three Oaks Parkway identified by the Village of Estero.
2. Estero Circulator and Treeline/Ben Hill Griffin Service Route linework derived from Lee County MPO 2040 LRTP Long Range Transit Map 4-2 Draft from November 2015.
3. Existing Routes and LinC linework source: Lee County GIS Data Catalog: Transportation.
4. Bike/ped/multi-use trail projects, fixed-route bus improvements and road/highway projects source: 2040 Transportation Plan Lee County MPO Needs Plan Projects.
5. The Village of Estero boundary is from Lee County government dated October 1, 2015.



LEE COUNTY SCHOOL DISTRICT FACILITIES

DATE
NOVEMBER 2017

PROJECT
20160267-000

FILE NO.

SCALE
AS SHOWN

MAP
PSF-1

Legend

Charter Schools

Elementary school

High school

Middle school

Special Center

Florida Southwestern State College

Florida Gulf Coast University

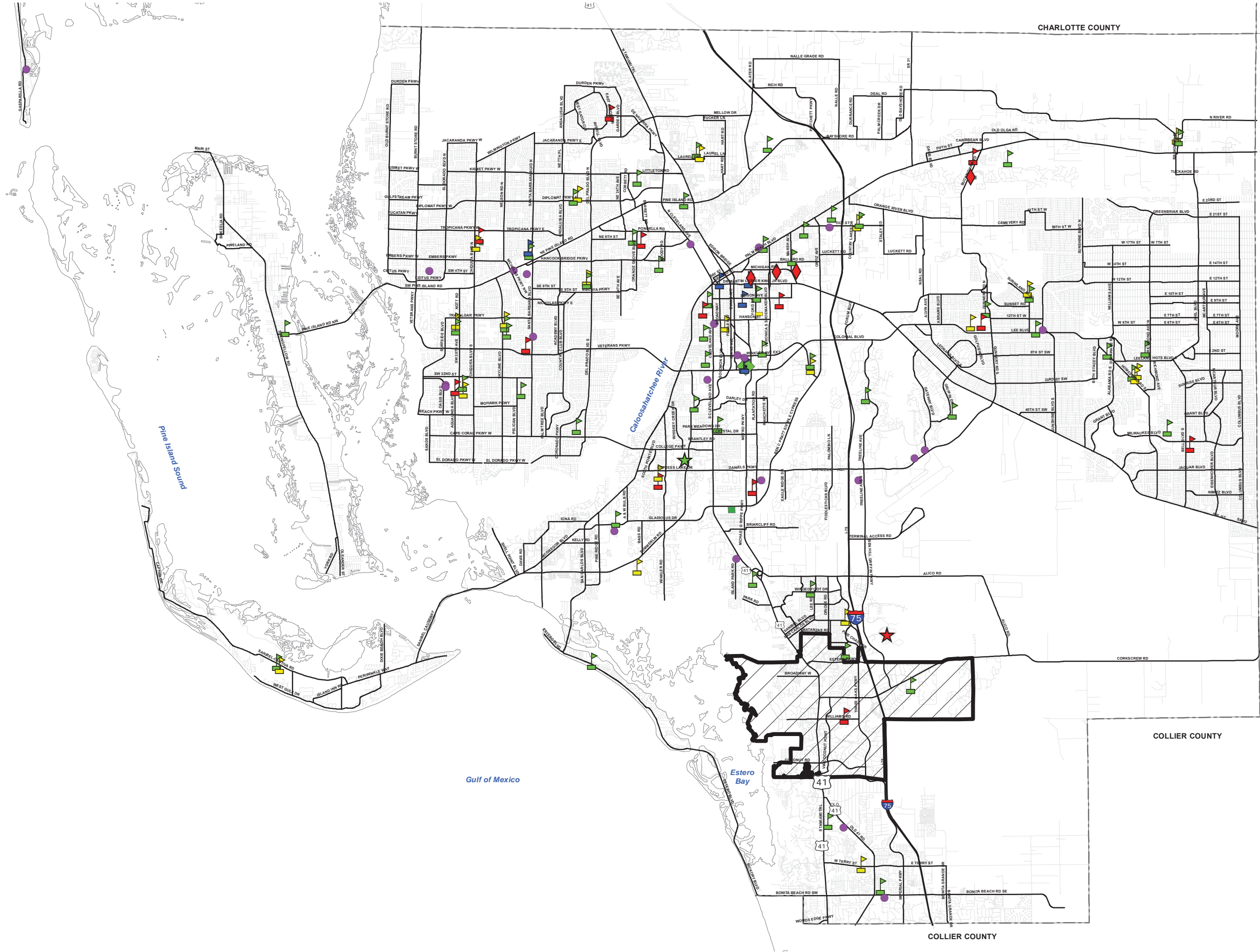
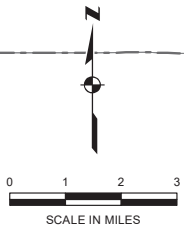
School Administration

Technical/Exceptional

Support Services

Transportation Facilities

Village of Estero



REVISIONS

NOTES

- The information shown is per Map 23 of the Lee Plan dated August 2017, adopted per Lee County Ordinance No. 17-19 (CPA2017-00003).
- The Village of Estero boundary is from Lee County government dated October 1, 2015.



**SCHOOL CONCURRENCY
 SERVICE AREAS**

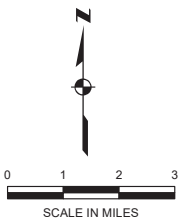
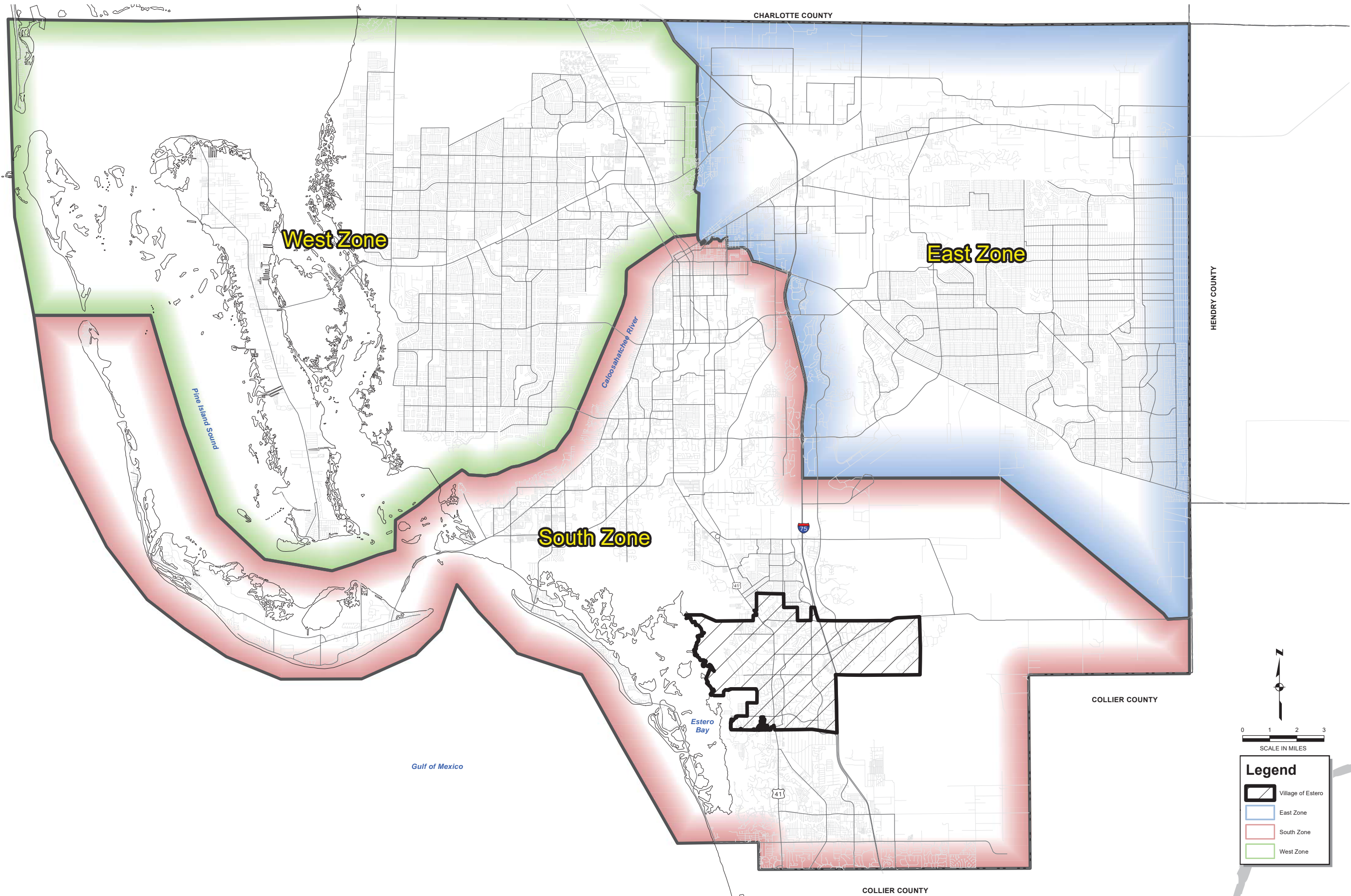
DATE
 NOVEMBER 2017

PROJECT
 20160267-000

FILE NO.

SCALE
 AS SHOWN

MAP
 PSF-2



Legend

- Village of Estero
- East Zone
- South Zone
- West Zone

NOTES

1. The information shown is per Map 23 of the Lee Plan dated August 2017, adopted per Lee County Ordinance No. 17-19 (CPA2017-00003).
2. The Village of Estero boundary is from Lee County government dated October 1, 2015.



REVISIONS