LAND DEVELOPMENT CODE

Addendum of Recommended Changes January 13, 2021

The attached document "Addendum of Recommended Changes" dated January 5, 2021 includes changes suggested from public input and interested parties (letters and Ecomments) as well as suggestions by the Design Review Board, Planning and Zoning Board, and staff.

The changes are listed by Chapter. The changes are highlighted with strike through in red and new language underlined in blue.

Each proposed change contains a "comment" box shown in grey which identifies the source of the comment (staff, public, etc.) The majority of comments received relate to technical details for landscaping standards. These changes were suggested primarily by current and former Design Review Board members. Other changes include:

- Technical road right-of-way details (staff clean-up revisions)
- Road cross-section illustrations (staff clean-up revisions)
- Minor clarifications of architecture
- Parking (staff/consultant)
- Additional mixed use standards (staff/consultant)
- School open space standards (staff/consultant)

Not every change suggest was incorporated, but many revisions are proposed.

Estero Land Development Code

<u>Addendum of Recommended Changes</u>

Note on changes:

- Deletions are shown in red strikethrough.
- Additions are shown underlined in blue.

Chapter 1: GENERAL PROVISIONS

1. Comment

Sec. 1-805 Development Approvals and Permits Under Prior Land Development Codes: Staff recommends changes to promote consistency with Sec. 1-804 and promote compliance with the new LDC.

Change

Sec. 1-805. Development Approvals and Permits Under Prior Land Development Codes

- A. All development approvals or permits approved before ___ [insert effective date of this LDC], either by the Village or by Lee County (prior to the incorporation of the Village), remain valid until their expiration date, and may be carried out in accordance with the terms and conditions of their approval, and the development standards in effect at the time of approval, as long as they remain valid and have not expired or been revoked or substantially modified. If the approval or permit expires or is revoked (e.g., for failure to comply with the terms and conditions of approval) or substantially modified, all subsequent development of the site shall comply with the procedures and standards of this LDC.
- B. Approval of a building permit prior to ___ [insert effective date of this LDC] means all plans and approvals for the development subject to the building permit are valid.
- C. By notifying the Director in writing, an applicant who has received approval of a master concept plan in accordance with the transitional LDC in effect before ____ [insert effective date of this LDC] may opt to have the proposed development reviewed and decided in accordance with the regulations in effect at the time the master concept plan was approved. Notification by the applicant shall be made within 90 days of ____ [insert effective date of this LDC] and shall be irrevocable.
- D. Any re-application for an expired development approval or permit shall comply with the standards in effect at the time of re-application.

Chapter 2: ADMINISTRATION

2. Comment

Table 2-405.B Summary of Public Notification Requirements – Staff identified an error in the table note numbering

Change

TABLE 2-405.B: SUMMARY OF PUBLIC NOTIFICATION REQUIREMENTS				
Application Type	Noti	Notice Required		
Application Type	Published	Published Mailed		
[]				
	Relief			
[]				
Appellant mails notice of public notice of public hearing at least 14 calendar days before Village Council hearing. Appellant mails notice of public hearing at least 14 calendar days before Village Council hearing.				
Note:				
1. If Council takes jurisdiction, it is responsible for notice.				

3. Comment

Sec. 2-502(C) Plat Review: Staff recommends the lists of exceptions for plat standards in 5-902 Applicability and 2-502(C) Plat Review be revised for consistency.

Change

C. Plat Review

1. Generally

This subsection supplements the procedural requirements of Ch. 177, Fla. Stat. for review and decisions regarding plats and replats. It applies to all subdivision of land, except the following:

- A. A development platted or approved by Lee County prior to January 28, 1983, provided that all required improvements have been made or that a security for the performance of the improvements has been posted and is current.
- B. The division of land for the conveyance of land to a federal, state, County or municipal government Village entity, or a public utility.
- C. The division of land by judicial decree.
- D. A division of land of two or fewer lots <u>out of a parent parcel</u> approved as a limited development order (Sec. 2-502.B.2, Limited Development Order Procedure)

E. A single family residential lot created between January 28, 1983, and December 21, 1984, that has obtained a favorable minimum use determination in accordance with the Lee Plan.

Chapter 3: ZONING DISTRICTS

4. Comment

Table 3-203.C AG District Uses – Staff recommends allowing schools without requiring a special exception in the AG district

Change

TABLE 3-203.C: AG DISTRICT USES [1][2] P = Permitted; S = Permitted by approval of a Special Exception; E=Existing Only			
Use Type Permission Use Specific Standard Principal Uses			
	n	0 1100 0 1100	
Agricultural use P Sec. 4-103; Sec. 4-103			
[]			
Schools, elementary, middle, or high Sec. 4-102; Sec. 4-135			
[]			

5. Comment

Table 3-703.D.1.A Maximum Gross Nonresidential Floor Area Allowed in RPD – A public comment indicated it should be revised to allow creativity integrating neighborhood mixed-use...one of VOE comprehensive plan goals. Should be increased to up to 5,000 sq ft in 100 units, up to 10,000 sq ft in 101-300 units, up to 15,000 sq ft in 301-600 units and up to 20,000 sq ft in 601-1200 units.

Change

TABLE 3-703.D.1.A: MAXIMUM GROSS NONRESIDENTIAL FLOOR AREA ALLOWED IN RPD [1]			
Total Approved Dwelling Units	Gross Nonresidential Floor Area Allowed (maximum square feet)		
Less Than 150 dwelling units 100 dwelling units or less	None-5000		
450101-300 dwelling units	2,500 <u>10,000</u>		
301-600 dwelling units	7,500 <u>15,000</u>		
601-1200 dwelling units	17,500 _20,000		
More than 1200 dwelling units	Additional space may be added at a rate of 5,000 square feet per 300 dwelling units to a maximum of 30,000 square feet. In no case may the commercial area exceed three percent of the gross area of the project.		

NOTES:

[1] The following uses shall not be counted against the maximum gross floor area in the RPD: EMS, fire, or sheriff's station; place of worship; school, elementary, middle, or high; day care; food or beverage, limited service; restaurant, convenience; restaurant, standard; drugstore; grocery store or food market; gas station (with self-service fuel pumps only), and home occupations.

6. Comment

3-706.C. Mixed Use Planned Development: Staff recommends modifying water body setback to 25 feet for consistency with setback requirement elsewhere in LDC

Change

C. Intensity and Dimensional standards			
Standard	Requirement		
Lot area (ft²)			
Lot coverage, max (percent of total area)			
Lot width, min. (ft.)	To be established in PD Master Concept Plan and as		
Lot depth, min (ft.)	set forth in Sec. Error! Reference source not		
Side setback, min. (ft.)	found., Error! Reference source not found The		
Rear setback, min. (ft.)	MPD District shall be consistent with the goals,		
Street setback (arterial street with frontage road) (ft.)	policies, and objectives in the comprehensive plan, and the FLUM.		
Street setback (arterial street without frontage road) or local street (ft.)			
Street setback (private street) (ft.)			
Intensity/Density	Consistent with comprehensive plan, and for lands designated Transitional Mixed Use, the bonus density standards (if applicable)		
Building height, max. (ft.)	45		
Waterbody setback	2550 feet from the edge of the waterbody		

7. Comment

3-707.B.6 Lots and Blocks: Due to the substantial benefits reaped from "alley-loaded" design in achieving VOE's Village Center goals, a public comment recommended "alley-loaded" design be much more prominently recognized as a highly-desirable land planning feature and valuable incentive to VOE.

Change

Sec. 3-707.B.6 Lots and Blocks

The proper application of the principles regarding connectivity of streets will then create a network of local streets which will result in varieties of blocks of land and differing sized lots within such blocks. Rather than specify a particular grid of lots and blocks which may differ within the tiers, variations in the size and dimensions of lots and blocks are permitted so long as the overall objectives for connectivity and walkability are achieved on a performance-based standard. However lots or blocks may be ultimately designed, the sides form a part of the public space and will be defined by the types and varieties of streets that surround them. By way of example, alleys can absorb parking and service loads, and allow the outer faces of blocks to become more intensely pedestrian in nature. These alley-loaded designs are highly desirable in the Village. Within the blocks that are created by the streets, a variety of widths and depths of individual lots will determine the range of building types and densities that will eventually establish the intended urban fabric of the Village Center Area.

3-707.D.Tiers: Staff recommends changes to promote compliance with the new LDC.

Change

3-707 D.Tiers

- 1. This subsection provides standards for four levels of development that will contribute to a walkable mixed-use environment in the Village Center Area: [...]
- 5. As to any development parcel in the Village Center Area with vested rights to develop under an approved and unexpired Master Concept Plan, nothing in this section shall affect or limit the right of the owner of such parcel to seek an amendment to the Master Concept Plan under the provisions of the LDC in existence prior to June 22, 2016, and in accordance with the provisions of Sec. 2-501.D, Planned Development), through a minor planned development application or, if applicable, by an administrative amendment in accordance with Sec. 2-506.C.2, Administrative Deviation, provided, however, that such amendment seeks no increase in density beyond ten percent of vested dwelling units or an increase in intensity.

9. Comment

3-707.R: a public comment requested for clarity of interpretation, the tier incentive offers should include language to make clear that lower tier incentives are available to projects that provide higher tier elements (e.g., for Tier 3 projects, the incentive offers from Tiers 1 and 2 are also available)

Change

Sec. 3-707.R.6. Tier 2 Incentive Offers

<u>In addition to Sec. 3-707.R.5, Tier 1 Incentive Offers, the following incentive offers are available:</u>

A. Public Hike/Bike Trails (High Value). Public Hike and Bike Trails, either on or off-site received one of the highest levels of public support in the Village.

[...]

Sec. 3-707.R.7. Tier 3 Incentive Offers

<u>In addition to Sec. 3-707.R.5, Tier 1 Incentive Offers, and Sec. 3-707.R.6, Tier 2 Incentive Offers, the following incentive offers are available:</u>

A. On-Street Parking (Medium-High Value). The public benefit of broad connecting streets with traffic calming and the feeling of relative safety by pedestrians from moving vehicles created by a barrier of on-street parked cars is seen as a very highly desirable public benefit.

[...]

Sec. 3-707.R.8. Tier 4 Incentive Offers

In addition to Sec. 3-707.R.5, Tier 1 Incentive Offers, Sec. 3-707.R.6, Tier 2 Incentive Offers, and Sec. 3-707.R.7, Tier 3 Incentive Offers, the following incentive offers are available:

A. Vertical Mixed-Use (High Value). Development plans with vertical mixed-use where non-residential uses such as office, retail, or dining are at the ground level and residential uses above.

[...]

Chapter 5: SITE DEVELOPMENT STANDARDS

SECTION 5-2: OFF-STREET PARKING, BICYCLE PARKING, AND LOADING STANDARDS

10. Comment

Table 5-204B-1: Minimum Number of Off-Street Parking Spaces

- Staff recommended including Multiple-Occupancy Complex
- A public comment requested "brew-pub" be revised to between the requirements for a restaurant and for a bar
- Other minor adjustments are recommended by staff

Change

TABLE 5-204.B.1: MINIMUM NUMBER OF OFF-STREET PARKING SPACES				
Use Type	Minimum Number of Vehicular Parking Spaces			
Agricultural use	No minimum			
[]				
Brewpub or micro-brewery, -winery, or -distillery	4014/1,000 sf of seating area			
[]				
Clubhouse (as accessory to golf course use)	6 per hole , or 14/1,000 sf, whichever is greater			
[]				
Multiple-occupancy complex	4.5 spaces per 1000 sf or 4.0 spaces per 1,000 sf for complexes larger than 500,000 sf			
[]				
Office, <u>general</u> contractor's or general business	1/300 sf			
[]				
Retail sales (not multiple occupancy)	1/ <u>350</u> 400 sf			
[]				
Wireless Telecommunications	1			

5-204.A.3.B: A public comment recommended it should read "...also serving as garage driveways of single-family homes, two-family dwellings or low-rise multi-family dwellings of 10 units or less..."

Change

Sec. 5-204.A.3.B. Backing onto Streets Prohibited

All off-street parking and loading areas shall be arranged so that no vehicle is required to back out from such areas directly onto a road or accessway, except for parking areas serving <u>as driveways of</u> single-family detached <u>dwellings</u>, or two-family dwellings, or coach homes with garages.

12. Comment

5–204.A.5: A public comment requested "asphalt paving" be replaced with "street surface".

Change

Sec. 5-204.A.5. Drainage

All off-street parking and loading areas shall be properly drained to eliminate standing water and prevent damage to adjacent land and public streets and alleys. The slope of asphalt paving the street surface shall be one percent or greater, and the slope of gutters shall be 0.3 percent or greater.

13. Comment

5–204.B.5.B: A public comment noted EV charging stations should not be required for small development. Minimum should be 60 parking spaces (i.e., a small strip mall).

Change

Sec. 5-204.B.5. Electric Vehicle (EV) Charging Stations

- A. Up to ten percent of the required number of off-street parking spaces may be used and designated as electric vehicle (EV) charging stations. The Director shall have authority to approve the use and designation of additional required parking spaces as electric vehicle charging stations, provided that such additional spaces shall count as only one-half of a parking space when computing the minimum number of parking spaces required. Parking spaces used as electric vehicle charging stations shall consist of one or more group(s) of contiguous spaces located where they can be readily identified by electric vehicle drivers (e.g., through directional signage), and where their use by non-electric vehicles is discouraged.
- B. An off-street parking area with more than 25 60 parking spaces and less than 150 parking spaces shall provide at least one EV charging station.
- C. An off-street parking area with 150 or more parking spaces shall provide at least three EV charging stations.

Section 5-3: Mobility and Connectivity Standards

14. Comment

5- 305.B.1.B: A public comment noted it should read "...a cross-access agreement with the landowners of the adjoining lot..."

Change

Sec. 5-305.B.1. Pedestrian Connectivity

1.B. If the applicant is unable to execute a cross-access agreement with the landowners of the adjoining lot, pedestrian circulation shall still be oriented to facilitate future cross-access connections with adjacent parcels. Abutting properties developed or redeveloped at a later date shall at that time be required to enter into the requisite cross-access agreement.

15. Comment

5–306.B.2: A public comment noted it should read "...cross-access agreement with the landowners of the adjoining parcels...."

Change

Sec. 5-306.B. Bicycle Connectivity Between Developments
2. If the applicant is unable to execute a cross-access agreement with the landowners of the adjoining parcels, bikeways shall still be oriented so as to facilitate future cross-access connections with adjacent parcels. Abutting properties developed or redeveloped at a later date shall at that time be required to enter into the requisite cross-access agreement.

SECTION 5-4: LANDSCAPE STANDARDS

16. Comment

5-401 Purpose: DRB suggested purpose of the landscaping should be better integrated with its relationship to a building's architecture.

Change

5-401 Purpose

A. The purpose of this section is to establish landscaping standards that integrate landscape, hardscape, and open space standards to:

- 1. Enhance the appearance of the Village by integrating landscape design elements with architectural forms and variable site relationships;
- 2. Provide trees for shade and heat and glare reduction;
- 3. Support soil erosion control and water conservation;
- 4. Screen incompatible land uses <u>and parking lots</u>, and <u>differentiate</u> incompatible architectural relationships and site elements; and
- 5. Improve air quality.

5-403.B.3: DRB suggested that plant separation requirements should not be dictated by simple numeric standards, which limits a design and is arbitrary, allow for building arrangement to dictate plant placement.

Change

Sec. 5-403.B.3. Trees and Palms

All required trees shall be a minimum 12-foot in height measured from the adjacent grade of the development site, with a sixfive-foot spread and threetwo and one half-inch caliper and forty-five gallon container size or field grown at the time of planting. Palms shall have a minimum of ten feet of clear trunk at planting. Trees adjacent to walkways, bike paths, and rights-of-way shall be maintained with eight feet of clear trunk. Trees shall provide 15-foot clearance from light poles and buildings; palms shall provide an eight-foot clearance. Trees shall provide a six-foot clearance from hardscapes; palms shall provide a four-foot clearance. Root barriers are required for all trees planted within ten feet of hardscape or utilities. See Table 5-403.B.3: Tree and Palm Plant Material Standards.

10

TABLE 5-4	03.B.3: TREE AND PALM PLANT MATE	RIAL STANDARDS
Standard	Trees	Palms
Height, spread, caliper at planting	12 feet in height, 65-foot spread, 32.5-inch caliper	10 feet of clear trunk
Clear trunk adjacent to walkways, bike paths, and right of ways	8 feet of clear trunk	10 feet clear trunk
	BIKE DATE!	PATH/ KWAY/ ADWAY
Clearance from light poles and buildings	15 feet	8 feet
Clearance from hardscape s	6 feet	4 feet
	15' MIN. CLEAR 15' MIN. CLEAR 8' MIN. CLEAR	SLEAR 8' MIN. CLEAR 9 MIN. CLEAR 4' MIN. CLEAR HARDSCAPE
Root barrier	Required within 10 feet of hardscape or utility	n/a

5-403.B.5. Shrubs: Bill Prysi commented that instead of dictating height of shrubs at installation, focus on height after a growing season to better insure the design fits within context of setting

Change

Sec. 5-403.B.5. Shrubs

Shrubs shall be a minimum of 24 inches (48 inches for Type F Buffers) in height, at time of planting. Saw palmettos (*Serenoa repens*) and coonties (*Zamia floridana*) may be used as shrubs, provided they are 12 inches in height at time of planting. All shrubs shall be-a minimum three-gallon container size and installed at an appropriate height to meet the minimum specified height after one growing season or one year from time of installation, whichever is more restrictive and be spaced an average of 18 to 36 inches on center. They shall be at least 36 inches (60 inches for type F Buffers) in height within 12 months of time of planting, and maintained in perpetuity at a height of no less than 36 inches (60 inches for Type F Buffers). Shrubs shall provide be installed to maintain an 18-inch clearance from hardscapes and 36-inch clearance from buildings.

19. Comment

5-403.B.6. Mulch Requirements: DRB commented that the Village does not want cypress mulch and some recycled mulch can be non-organic, which the Village does not want as a standard.

Change

Sec. 5-403.B.6. Mulch Requirements

A two-inch minimum layer, after watering-in, of mulch or other recycled organic mulch materials shall be placed and maintained around all newly installed trees, shrubs, and groundcover plantings. Each tree shall have a ring of mulch no less than 24 inches beyond its trunk in all directions. The use of cypress mulch is strongly discouraged prohibited.

20. Comment

5-404.C.2.F exemption to tree preservation should be clarified to apply to single family lots and not just the RSF district

Change

5-404.C.2.F

The removal of trees on a lot in the RSF district zoned for single-family residential use or that is being used lawfully by a single family dwelling or mobile home where the residence or proposed residence is located on a lot no greater than five acres in area.

5-404 Tree Preservation: Incorporate flexibility in process for how trees are to be replaced, in certain situations two larger trees is more appropriate than three trees.

Change

Sec. 5-404.F. Restoration Standards

If a violation of this section has occurred and upon agreement of the director and the violator, or, if they cannot agree, then upon decision by a Special Magistrate, a restoration plan must be ordered in accordance with the following standards:

- 1. The restoration plan must include the following minimum planting standards:
 - A. The plan must include a planting plan for all protected trees. Replacement stock must be computed on a three for one basis according to the total number of unlawfully removed trees. The phrase "three for one" in this section refers to the requirement of replacing an illegally removed tree with three live trees according to the provisions of this section. Replacement trees must be nursery grown, containerized. and sized to satisfy the standards of Sec. 5-403.B.3 above. It is within the discretion of the Director to allow a deviation from the ratio specified in this subsection. When such deviation is sought, the total of heights and calipers must equal or exceed that specified in the standards set out in this subsection. An example of this might be one in which trees four feet in height might be planted in a ratio of five replacement trees to one illegally removed tree. Justification for such a deviation must be provided to the Director. Where situations create an inability to meet the three to one (3:1) replacement standards, a two to one (2:1) replacement may be applied so long as the replacement trees are 16fet in height (16'H), eight feet in spread (8'S), four inch caliper (4:CAL), and one-hundred gallon (100G) in size. Palms must be sixteen-feet clear trunk (16'CT).

22. Comment

5-405B General Tree and Street Tree Standards: Bill Prysi suggested 3 inch caliper should not be associated with 12 foot trees, not an industry standard.

Change

Sec. 5-405.B. General Tree Standards

Development subject to the requirements of this subsection shall comply with the requirements of Table 5-405.B: General Tree Standards. All required trees shall be a minimum of 12 feet in height measured from final grade of the project site, with a six-foot spread-five-foot spreads (5'S) and have three-inch calipers at dbh_two and one half inch caliper (2-1/2" CA) and forty-five gallon (45G) or field growth (FG) in size at the time of planting, except that palms shall have a minimum of ten feet of clear trunk at time of planting

5-405. C. Offsets and Credits: DRB commented that credit should not be given for spartina.

Change

Sec. 5-405.C.1 Credits

. . .

F. Credit shall not be given for spartina.

24. Comment

5-405C.3.C: Alternative Landscape Betterment Plan: Bill Prysi commented that standard should never require 100 percent native species, should be same as native species requirement of the proposed code.

Change

Sec. 5-405.C.3. Alternative Landscape Betterment Plan C. One hundred (100) percent of the required trees installed shall be native species. The betterment plan shall not deviate from the native species requirements of this LDC.

25. Comment

5-406.B Buffers: During DRB workshop to review proposed LDC, comments were made regarding buffers on Right-of-Way and how they inhibit the architecture of a building, and that there should be flexibility regarding when the buffer is required to screen an incompatible use and when the use should be seen from the R-O-W.

Change

TABLE 5-406.B.3.: BUFFER TYPES (PER 100 LINEAR FEET) [1]							
Ctondord		Buffer Types					
Standard	Α	В	С	D	E	F	
Minimum Width in Feet	Five	15	20	20	30	50	
Minimum Number of Trees (per 100 linear feet)	Four	Five	10	Five [4]	10	15	
Minimum Number of Shrubs (per 100 linear feet)	_	Hedge [3]	30	Hedge [3]	30	Hedge [3]	
Wall Required [2]	_	_	Eight feet in height,	_	Eight feet in height,	_	

TABLE 5-406.B.3.: BUFFER TYPES (PER 100 LINEAR FEET) [1]						
Ctondond	Buffer Types					
Standard	Α	В	С	D	E	F
		solid fence fence				

NOTES

- [1] All landscape buffer designs should complement adjacent project buffers to help in establishing a continuous landscape theme within the Village.
- [2] A solid masonry or concrete wall, berm, or wall and berm combination shall be at least eight feet in height. All trees and shrubs required in the buffer shall be placed on the residential side of the wall (if applicable). The height of the wall shall be measured from the average elevation of the road or roads abutting the property, as measured along the centerline of the roads, at the points of intersection of the roads with the side lot lines (as extended) and the midpoint of the lot frontage. Walls shall be constructed to ensure that historic flow patterns are accommodated and all stormwater from the site is directed to on-site detention/retention areas in accordance with SFWMD requirements.
- [3] Hedges shall be planted in double staggered rows and be maintained to form a 36-inch high (F type buffers shall be 48 inches at installation and be maintained at 60 inches high) continuous visual screen within one year after time of planting. In situations where the elevation of the ROW is higher than the elevation of the adjacent property, the effective plant screen shall have an elevation of 36 inches as measured from the highest elevation within the buffer area resulting from the combination of the berm and/or plants. Clustering of shrubs that would not create a continuous visual screen, but would add interest to the landscape design, is allowed, subject to review by the Director.
- [4] Trees within the ROW buffer shall be appropriately sized in mature form so that conflicts with overhead utilities, lighting, and signs are avoided. The clustering of trees and use of palms within the ROW buffer will add design flexibility and reduce conflicts.
- [5] Deviations are allowed for a reduced or eliminated roadway buffer where a building is designed within 15' of the right-of-way in an urban context, subject to the same review and approval process identified for alternative landscape betterment plans.
- [6] Deviations are allowed for a reduced or eliminated roadway buffer where a lake is along a right-of-way, to accomplish strategic views with clustering or placement of plantings around the lake. This is subject to the same review and approval process identified for landscape betterment plans.

26. Comment

5-407.C. Building Perimeter Planting Standards: DRB suggested that building perimeter standards be reviewed to provide a higher requirement with more flexibility

15

Change

Sec. 5-407.C. Building Perimeter Planting Standards

The planting areas for building perimeter plantings shall be located abutting three sides of a building, with emphasis on the sides most visible to the public (not including the loading area). The perimeter planting area shall consist of landscape areas, raised planters, or planter boxes that are a minimum of five ten feet wide. These planting areas shall include shrubs and ground cover plants with a minimum of 50 100 percent coverage of the planting area at the time of planting. Trees and shrubs shall comply with the size standards of this subsection. Groundcover plants shall be a minimum one-gallon container size at installation and spaced not less than 24 inches on center. General trees may be planted within the building perimeter planting area; especially effective are clusters (three or more) of sabal palms. Turfgrass is discouraged and is limited to ten percent of the planting area considered acceptable for compliance with minimum Building Perimeter Plantings. Water management areas may not be a part of the planting area. Pedestrian accessways may cross and loading areas may be placed in the perimeter planting area, but may not be used to meet minimum planting area or open space requirements.

Sec. 5-407.D. Shopping Centers

An enlarged perimeter planting area is required in the front of shopping centers and freestanding retail development uses that constitute a large development (a project of ten acres or more in land area or two acres or more in impervious area). An area that is at least five percent of the size of the parking area shall be developed as green space within the front of shopping centers and other retail establishments and be an enlargement to the front building perimeter planting area. It is not a requirement that this area directly abut the front of the building. The enlarged perimeter planting areas shall consist of landscape areas, raised planters, or planter boxes that are a minimum of five ten feet wide. These enlarged perimeter planting areas shall include trees, shrubs, and ground cover plants with a minimum of four trees per 100 linear feet of building and 50 100 percent coverage of the landscape area at the time of planting. The trees placed around the building shall be applied to the general tree requirement. Trees and palms may be installed in clusters and do not need to be located within a 100 linear foot segment. Clusters of trees and palms at the corners of buildings or framing entrances are especially effective. Trees and shrubs shall comply with the size requirements of this subsection. Groundcover plants shall be a minimum one-gallon container size. Taller palms (16-to 20-foot clear trunk) shall be used when building height is greater than 35 feet. Turfgrass is discouraged and is limited to ten percent of the landscape area. Water management areas shall not be a part of this enlarged planting area. Decorative paving areas incorporating courtyards, walkways, water features, plazas, covered seating and outdoor eating spaces may be used to meet up to 20 percent of the required building foundation planting area.

5-408. Open Space and Stormwater Management Areas: DRB commented that the Village should prioritize outdoor active and public spaces to promote gathering, and thus increase the percentage allowed of these amenities to count to open space requirement.

Change

Sec. 5-408.D. Open Space Calculation

The following features on a site shall count toward the open space standards of this subsection, if the minimum dimensions are met:

- 1. Native and indigenous preservation areas, where such vegetation is onsite.
- 2. Outdoor active and passive public use areas such as plazas, atriums, courtyards, and other similar space, up to a maximum of 2550 percent of the required open space. These areas when used with those noted in subsection 6 below may not combine for more than 50 percent of the overall required open space for the site.
- 3. Buffers and vehicular use area landscaping.
- 4. Dry detention areas.
- 5. Existing or proposed bodies of water, including stormwater management areas and areas subject to saltwater inundation, up to a maximum of 25 percent of required open space.
- 6. Active and passive recreation areas, such as playgrounds, golf courses, nature trails, bikeways, pedestrian ways, tennis courts, swimming pools, and other similar open spaces, if no more than 2050 percent of the recreational area credited as open space consists of impervious surface. These areas when used with those noted in subsection 2 above may not combine for more than 50 percent of the overall required open space for the site.
- 7. Archaeological sites that are designated as significant historic resources.

28. Comment

Table 5-408.C: Open Space Standards - Staff recommends adding 20% open space for schools to the table

Change

TABLE ERROR! REFERENCE SOURCE NOT FOUND.: OPEN SPACE STANDARDS			
Type of Development	Open Space as Percentage of Development Area [1]		
Type of Bevelopment	Small Project [2]	Large Project [2]	
Residential			
Single-family or mobile home dwelling on a single lot where minimum lot size is 6,500 sq. ft.	None	None	
Two-family dwelling on a single lot where minimum lot size is 7,500 sq. ft.	None	None	
Two-family attached each on an individual lot where minimum lot size is 3,750 sq. ft. per unit	None	None	
All other Residential, including ALF, independent living, continuing care, and multifamily	35	40	
Lee County School District schools	<u>20</u>	<u>20</u>	
Other: All other uses including, but not limited to commercial, industrial, places of worship, recreational vehicle parks, community facilities, other schools schools (excluding Lee County School District schools), etc.	30	40	
Planned Developments	30	40	
NOTES:			

- [1] Multiple use sites with conventional zoning shall comply with each corresponding use percentage in this table.
- [2] A Large project is ten acres or more in land area or two acres or more in impervious area.

29. Comment

5-408D: During discussion of stormwater management area design by DRB, comment was noted that the proposed standard is the County standard; by contrast, the Village standard is not prescriptive and does not provide for a tree reduction when planting dry detention basins.

Change

Sec. 5-408.I.3. Dry Detention Basins

All dry detention basins shall be planted with wetland type plant species (such as spartina), in minimum one-gallon containers, not more than 36 inches on center, throughout the extent of the basin. For each 400 square feet of dry detention area or drainage swale planted with appropriate native herbaceous vegetation (minimum one-gallon container size planted three feet on center), the general tree requirement may be reduced by one ten-foot tree. Dry detention basins shall be designed to incorporate low-impact design standards

by integrating general tree and the required herbaceous basin plantings into their design.

30. Comment

5-408.I.6 Bulkheads, etc.: Comments at DRB suggested increasing the percentage allowed for riprap on shorelines, and adding more specific standards for littoral shelf planting

Change

Sec. 5-408.I.6. Bulkheads, Riprap Revetments, or Other Similar Hardened Shoreline Structures

Bulkheads, riprap revetments, or other similar hardened shoreline structures may comprise up to 2033 percent of an individual lake shoreline. A compensatory littoral zone equal to the linear footage of the shoreline structure shall be provided within the same lake and meet the following standards:

- A. A five-foot wide littoral shelf planted with herbaceous wetland plants that provides 50 percent coverage at time of planting. not less than liner size material planted not more than 18 inches on center or one-gallon material above the mean high water line not more than 36 inches on center. To calculate the littorals for this shelf design, the number of linear feet of shoreline structure shall be indicated, multiplied by five feet for the littoral shelf width, multiplied by 50 percent for the plant coverage at time of planting;
- B. An 8:1 slope littoral shelf with herbaceous wetland plants that provide 50 percent coverage at time of planting; or
- C. An equivalent littoral shelf design approved by the Director.

Sec. 5-408.I.8.C

Requirements that ensure littoral vegetation <u>and palms</u> remains in a healthy and vigorous state, in perpetuity. (The use of trimming, mowing, and herbicides to remove littoral plants are prohibited.)

31. Comment

Table 5-409.C: Landscaping Standards for Parking Areas: Bill Prysi commented additional flexibility should be incorporated within the parking area landscape standards

Change

TABLE 5-409.C: LANDSCAPING STANDARDS FOR PARKING AREAS			
Landscape Elements	Standards		
Minimum Landscape Area (Percent) (on the parking area perimeter or within internal islands)	Shall equal or exceed a minimum of 10 percent of the total paved surface area Fountains, seating areas, and similar features within a vehicular use area are allowed, and the space occupied by such feature(s) is allowed to off-set the required landscape area.		

TABLE 5-409.C: LANDSCAPING STANDARDS FOR PARKING AREAS				
[]				
Minimum Number of Trees and Palms	One canopy tree or a cluster of three sabal single trunk palms shall be planted or retained for every 250 square feet of required internal planting area.			
	One tree <u>or palms</u> per planting island and one tree per <u>or</u> planting area.			
	Canopy requirements shall be met with existing indigenous native trees whenever such trees are located within the vehicular use area (parking area).			
	Trees for parking lots shall not interfere with the visibility and movement of vehicles or pedestrians, or cause pavement or other hard surfaces to heave.			
	Material selection shall be designed to survive the effects of building or large paved areas in terms of heat, shade, wind, etc.			
[]				
Terminal Islands	A minimum ten-foot-wide terminal island is required at the end of all parking rows. ¹			
	Curbing is strongly encouraged required.			
Landscape Area Coverage	Internal landscape areas not dedicated to trees or to preservation of existing vegetation shall be landscaped with grass, ground cover, ornamental grasses, shrubs or other approved landscaping materials. Turf grass is not acceptable for internal landscape areas. This shall be noted on the landscape plans.			
	A maximum of ten percent can be sod.			
	Sand, gravel, rock, shell, or pavement are not appropriate landscape materials.			

¹ See Sec. 33-114 of the transitional LDC.

5-411.D. Installation of Landscape Materials: DRB suggested adding an option to plant in right-of-way when easement holder permits

Change

Sec. 5-411.D. Installation of Landscape Materials

All landscape materials shall be installed in a horticulturally correct manner. At a minimum, the following installation requirements shall be met:

1. All landscape areas shall be mulched unless vegetative cover is already established.

[...]

- 5. Utility or drainage easements may overlap required buffers. No required trees or shrubs shall be located in any utility or drainage easement unless a written statement, from the entity holding the beneficial interest in the easement, is submitted specifically stating that the entity has no objection to the landscaping and that the proposed landscaping will not interfere with the long-term maintenance of the infrastructure within the easement. No required landscaping shall be located in a road easement or right-of-way. To avoid conflicts with overhead utility lines, only trees less than 20 feet in height at maturity may be used directly adjacent to an overhead line. Variances or deviations from the requirements of this subsection are prohibited unless approved by the Village and the easement or ROW holder for the planting of street trees.
- [...]
- 8. If a wall or fence is proposed, but not required, then the required buffer plantings shall be installed on the exterior side (between the wall and the abutting property or street right-of-way) of the wall or fence.

33. Comment

Suggest change as shown:

5-413. IRRIGATION STANDARDS

C. Design Standards

10. All Irrigation Systems will be designed to separate low and high watering demand areas into separate zones.

Change

Sec. 5-413.C. Design Standards

1. All required irrigation systems shall be designed to eliminate the application of water to impervious areas, including roads, drives, and other vehicle areas.

[...]

10. All Irrigation systems will be designed to separate low and high watering demand areas into separate zones.

SECTION 5-5: FENCE AND WALL STANDARDS

34. Comment

Suggest changes as shown:

5-510. RESIDENTIAL PROJECT WALLS

C. 3. Where decorative aluminum railing type fencing is applied, 50 percent of the required landscaping can be placed on the inside of the fence for aesthetic purposes.

Change

Sec. 5-510. Residential Project Walls

C. Shall be landscaped on the exterior side (between the wall or fence and the adjacent property or street right-of-way) with a minimum of five trees per 100 lineal feet and shrub hedges, within a minimum plantable area that is at least seven and one-half feet wide located on the exterior side of the fence or wall.

- 1. Hedges shall be planted and maintained to form a 36-inch-high continuous visual screen within one year after time of planting.
- 2. Trees adjacent to a right-of-way shall be appropriately sized in mature form so that conflicts with overhead utilities, lighting, and signs are avoided. (The clustering of trees and use of palms adjacent to the right-of-way will add design flexibility and reduce conflicts.)
- 3. Where decorative aluminum railing type fencing is applied, 50 percent of the required landscaping can be placed on the inside of the fence for aesthetic purposes.

SECTION 5-7: ARCHITECTURAL, FORM, AND DESIGN STANDARDS

35. Comment

Multiple locations in Section 5-7: Bill Prysi suggested changes related to material standards, including the option for wood-looking alternatives throughout the section

Change

TABLE 5-703.A.1.D: MEDITERRANEAN REVIVAL STYLE CHARACTERISTICS		
Roof shape Hipped, gabled, or a combination of both.		
Roof slope	Roof slopes are shallow and are sloped between 3:12 and 6:12.	
Roofing materials	Barrel tile, Spanish "S" tile, or flat concrete tile.	
Roof overhangs	Vary from deep to having no overhang at all. When deep overhangs exist, they are supported by sizable wooden brackets. Roofs without overhangs are finished with a molded cornice.	

Exterior	Walls are stucco and colored with richness, variety, and multiple methods of application. Window and door surrounds are minimal and are made of stucco or stone.	
Brackets, balconies,	Usually wood, wood-look	
porches, shutters,	alternative or iron.	
and other elements		
Openings, windows	Arched, vertical, and/or square	
shape/structure	proportions. Occasional round,	
	oval, or ornamental window used	
	as a façade accent.	
	Windows have divided lights and	
	are commonly double-hung,	
	single-hung, or casement.	
Door position	Recessed, casting deep shadows	
	revealing the thickness and	
	solidity of the structure.	
External spaces	•	
	and courtyards are a common	
	element.	
	Loggias, a porch not attached but	
	located within the volume of the	
	building, are very common and	
	may even serve as outside	
0	circulation between rooms.	
Ornamentation	Columns, posts, wooden and	
	masonry balustrades, brackets,	
	arched openings, arcades, and	
	towers are all very common	
	elements.	

Sec. 5-703.A.2 Florida Vernacular

The Florida Vernacular architectural style includes these substyles within the Village as further described in Sec. 5-703.A.2.D: Florida Plantation, Florida Key West or Cracker, and Historic Koreshan.

- A. The Florida Vernacular style of architecture is native to the region. It is most typically constructed with a wooden frame and finished with wood or <u>wood-look</u> <u>alternative</u> siding. Stucco and fiber cement siding are also used.
- B Florida Vernacular features porches as integral to the style and prominent on the front façades. Porches extend along a large percentage of the ground floor elevations, often wrapping the corners to continue at some length alongside façades.
- C. Florida Vernacular Style architecture includes the key characteristics identified in Table 5-703.A.2.C: Florida

Vernacular Style Characteristics. See also Figure 5-703.A.2.C: Florida Vernacular Defining Elements.

705.A.Z.C. Florida Vernacular Delining Elements.				
TABLE 5-703.A.2.C: FLORIDA VERNACULAR STYLE				
CHARACTERISTICS				
Roof shape	Gabled			
Roof slope	Between 6:12 and 12:12			
Roofing materials	Standing seam or "V" crimp			
	metal, asphalt shingles,			
	wooden or wood-look			
	<u>alternative</u> shakes.			
Roof overhangs	Deep, between two and four			
	feet, with exposed rafter			
	tails. Fascias on the gabled			
	ends are deeper than those			
	exposed along the eaves.			
Exterior	Horizontal wood or wood-			
	look alternative lap-siding,			
	vertical board and batten,			
	wood <u>or wood-look</u>			
	alternative shingles. Siding			
	typically exposes four to six			
	inches to the weather, which			
	is terminated with corner			
	boards at building edges.			
	Stucco finishes are also			
	appropriate, though less			
	common. Modern day			
	building materials also			
	include fiber cement siding.			
	Brackets, balconies,			
	porches, shutters, and other elements are usually wood,			
	wood-look alternative or			
	iron.			
	IIOII.			

Sec 5-703.A.2.D.2 Florida Key West or Cracker
The Florida Key West or Cracker style includes a foundation
of wood or wood-look alternative posts, limestone, brick or
concrete piers; horizontal weatherboard or clapboard as the
primary exterior material; a low-pitched gable roof, with wood
or wood-look alternative shingles or pressed metal shingles;
full façade wrap-around porch; louvered vents, doors, and
window shutters; and centrally placed main entrance with
transom light above.

Sec 5-703.B.1 Alternative Styles

Prairie: The Prairie style includes low-pitched hipped or flat roof; broad overhanging eves; strong horizontal lines; clerestory windows arranged in horizontal bands; wide use of natural materials, especially stone and wood or wood-look alternative with strong horizontal lines; and restrained

ornamentation such as friezes around windows and doors, or as bands under the eaves.

Sec 5-703.B.2 Alternative Styles
Florida Key West or Cracker: The Florida Key West or
Cracker style includes a foundation of wood <u>or wood-look</u>
<u>alternative</u> posts, limestone, brick or concrete piers;
horizontal weatherboard or clapboard as the primary exterior
material; a low-pitched gable roof, with wood <u>or wood-look</u>
<u>alternative</u> shingles or pressed metal shingles; full façade
wrap-around porch; louvered vents, doors, and window
shutters; and centrally placed main entrance with transom
light above.

36. Comment

5-703.A Primary Architectual Sizes: A commenter at DRB noted the need to revise the narrative in the LDC around Mediterranean Revival architectual styles because not all example of that style are highly articulated or ornate.

Change

Sec. 5-703.A.1. Mediterranean Revival

The Mediterranean Revival architectural style includes these sub-styles within the Village as further described in Sec. 5-703.A.1.E: Mission Revival, Italian Renaissance Revival, Italian Countryside, Spanish Revival, and Spanish Colonial.

- A. The Mediterranean Revival style is highly articulated with varied massing and architectural features. Towers, balconies, loggias, porticos, chimneys, trellises, and exterior staircases are assembled to form picturesque buildings. The result is buildings that are rich in shade and shadow, with multiple building volumes and setbacks, and varied building heights.
- B. Building composition is typically asymmetrical. Base, middle, and top are defined by moldings, changes in window pattern and size, and cornice lines. Arcades and loggias are also commonly used to reinforce the base, middle, and/or top of the building.
- C. The Mediterranean Revival building is typified as ornate, asymmetrical, and eclectic. Columns, posts, wooden and masonry balustrades, and brackets are contributing elements of the style. It is common to have multiple building volumes and varied interior and exterior spaces. Building massing tends to be irregular with a variety of shapes and heights; however, the appearance of solidary and permanence is critical.

37. Comment

5-705C. Mixed-Use Development Design Standards: Staff and consultants recommend adding additional mixed-use development standards to clarify applicable criteria.

Change

5-705.C. Mixed-Use Design Standards

1. Placemaking

[...]

2. Additional Mixed-Use Development Standards

The following additional mixed-use development standards also apply to all mixed-use development in any District, to the extent set forth herein:

A. General Criteria

Wherever the standards and forms governing mixed-use development under this Code do not provide either a clear solution to an issue of interpretation or make a specific determination regarding an issue arising during the development approval process, the following general criteria shall be deemed to be guiding principles which shall be applied. The goal of such a process shall be to provide consistent standards to apply in order to be achieve the goals for mixed-use development as set forth in the Comprehensive Plan.

1. Accessibility

Public space shall be designed to be walkable and accessible to the users of the development and be part of an integrated system of sidewalks, pedestrianways, bikeways, civic spaces, and similar features, and designed to be easily accessed by pedestrians.

Accessibility shall be based on the primacy of the human scale over the automobile.

2. Streets

Streets shall be provided that are part of a connected, continuous street network which is designed to encourage and support mixed-use development. Where the mixed-use development is of the size to support a mixture of different types of development character and neighborhoods, different types of streets which connect the different neighborhoods and other types of development shall be used (to minimize the traffic load and the need for increased capacity on any one street). To the maximum extent feasible, streets shall not end in dead ends, cul-de-sacs, hammerheads, or other forms which do not connect with other streets.

3. Street Design

Where a new set of streets is developed, or the existing street system is modified, to the maximum extent feasible, ensuring the distances between street intersections and the general street design shall support traffic calming, and slow traffic at intersections to allow pedestrians to cross streets quickly. In addition, establishing a general street design that enhances walkability and pedestrian connections as well as street connections. Where appropriate, landscaped medians, narrower street width, and two-way streets are encouraged to achieve these criteria. On-street parking, where appropriate and feasible, is encouraged, to protect pedestrians from the actual and perceived danger of moving traffic.

4. Lots and Blocks

Where the mixed-use development is of sufficient size that it is appropriate to establish a network of local streets within the development, a variety of blocks should be designed, along with lots within those blocks. The variation in the size and dimensions of blocks and lots should be designed to support walkability, and strong pedestrian connections through the use of sidewalks, pedestrianways, bikeways, trails, street trees, the use of open space, and alleys (where appropriate). The outside of the blocks, which should be bordered by sidewalks, streets, and street trees, should form a part of the public space and should be defined by the types and varieties of streets within the development. Where appropriate, alleys should be used for parking and service loads, allowing the outer faces of blocks to become more intensely pedestrian in nature. The variety of widths and depths of individual lots within the blocks should determine the range of building types and densities that will eventually establish the intended mixed-use development fabric.

5. The Visual Edge

The sidewalks, setbacks, building façades, and other characteristics of the visual edge of a street that is publicly accessible to the mixed-use development is also important in establishing the character of the mixed-use development. The height of the buildings, setbacks, and projections along the street define the enclosure of the street. The maximum width and height of buildings define a building's mass, while the architectural features of the building, especially the interrelationship of the design and the public space will ultimately determine the vitality of the street. So too are the characteristics of built form and landscape design which are deemed to be mutually dependent. All these factors should be considered in the design of the buildings and their relationship to the visual edge of a street.

6. Architecture

Architectural variety of buildings in the mixed-use development, and unique approaches to design and structure are valued and should be considered in the design of the buildings within the development. Also important are the adjacent buildings and public spaces. They shall be considered in the design of the buildings, and where appropriate, the design of the buildings within the mixed-use development should share some of the characteristics of its neighbors to create a cohesive framework.

7. Quality of Buildings

Buildings within a mixed-use development are like permanent fixtures in the landscape of the Village. They should be constructed with sufficient material and high technical quality to allow for their continuing renovation and adaptive reuse well beyond the expiration of their initial planned use or cost recovery. Building design and construction are encouraged to be cognizant of

southwest Florida's unique climate, and ecologically sensitive in their use of materials, particularly recyclables, and with respect to their energy demands.

B. Pattern Books Specific to Mixed-Use Development

In addition to the requirements of Sec. 3-702.D.3, the Pattern Book prepared should also include the following:

1. Illustrative Site Plan

An illustrative site plan, that includes the following additional information:

- (a) The location, shape, and size of proposed detention and retention areas.
- (b) The location and size of development tracts, labeled with approximate acreages and with proposed uses.
- (c) <u>The location and cross-sections of streets, sidewalks, and off-street facilities for walking or biking.</u>
- (d) The configuration and phasing of all connecting streets. This should also include the streets behind/between outparcels and other planned local streets, along with all access points from adjoining streets, as shown on the development plan with cross-sections for each.
- (e) A three-dimensional diagram or rendering that shows the scale and massing of buildings proposed in each development tract.
- (f) The location and size of common parking areas.
- (g) The location and approximate size of lots.
- (h) The landscaped areas and buffers, preserved areas, open spaces, civic spaces, gathering places, natural and cultural resources, and community facilities, where applicable.

2. Façade Detailing

Typical façade detailing for all sides of all buildings.

3. Pad Sites or Outparcels Developed Separately

Where pad sites or outparcels are to be developed separately, a plan with detailed examples, figures, or photographs that indicates what unifying themes will be common to those sites (architecture, signage, landscaping, etc.).

4. Tracts, Blocks, or Parcels Controlled for Security

For tracts, blocks, or parcels where access would be controlled for security, a plan diagram that shows what land would have controlled access, the proposed method and extent of access control along with the features thereof, and architectural elevations that depict the appearance of the controlled area from the outside of such parcel. The access plan diagram shall also show the layout of the vehicular, pedestrian, and bicycle network, the proposed operation of the access control features, and the proposed locations of sidewalks, trails, bicycle paths, drives, streets, fencing, gates, and walls, and their role in the security for such areas.

2.3. Crime Prevention Through Environmental Design (CPTED)

[...]

3.4. Climate Responsiveness

[...]

4. Tier Applicability

A. Horizontal Mixed Use shall comply with the Tier 1 standards as specified withing Sec. 3-707.D.1.A.

B. Vertical Mixed Use shall comply with the Tier 2 and Tier 3 standards as specified within Secs. 3-707.D.1.B and 3-707.D.1.C.

38. Comment

5-706.C Building Entries and Façades: During DRB workshop, comment suggested allowing use of overhead doors in light of current trends in restaurant design, so long as the doors are designed to look like windows

Change

Sec. 5-706.C Building Entries and Façades

- 1. The main entrance of the structure shall be oriented toward the public right-of-way on which the structure fronts. On a corner lot or site, the main entrance may be oriented to either the road or the corner.
- 2. For compliance with building design standards, exterior façades of outparcel buildings shall be treated as primary façades and employ architectural, site, and landscaping design elements that are common to the theme used in the main development on site, including colors and materials associated with the main building. The purpose of this requirement is to assure a unified architectural theme and site planning between out-parcels and the main buildings on site, enhance visual impact of the buildings and to provide for safe and convenient vehicular and pedestrian access and movement on site.
- 3. For compliance with building design standards, all sides of a building are subject to architectural design elements.
- 4. Overhead doors facing a public right-of-way are prohibited, with the exception that overhead doors may face an alley <u>or overhead doors</u> which utilize framed transparent glass panels covering a minimum of 75 percent of the door area shall be allowed on primary façades.
- 5. Blank wall areas shall be interrupted with an opening or relief work including one or more of the following:

39. Comment

Sec. 5-706.J. Lighting: At DRB workshop, some members approved of the decorative light pole requirement, while others preferred that light poles be designed to blend into the design of the building. Compromise was to state the poles should complement the project's design.

Change

Sec. 5-706.J. Lighting

- 1. Building entryways shall be lighted at the pedestrian level, with features such as lighted bollards or doorway lighting.
- 2. Light fixtures shall complement the overall development.
- 3. Decorative light poles and fixtures shall be used throughout all parking areas. Light poles and fixtures should complement the architecture of the development Except for pedestrian light fixtures, all outdoor light fixtures shall be fully shielded.
- 4. Lighting plans shall be coordinated with landscape plans to identify and eliminate potential conflicts with required landscaping.
- 5. No light poles shall be located in parking area islands that contain required landscaping.
- 6. The following lighting designs are prohibited with the exception that temporary seasonal lighting during the months of November and December is excluded from these prohibitions:
 - A. Buildings, awnings, roofs, windows, doors and other elements may not be outlined with light.
 - B. Exposed neon or LED tubing.
 - C. Backlit awnings.

40. Comment

5-707 D.6 Automobile Service Station / Canopies: At DRB workshop, it was noted that the standard in the draft would allow the single sloped WAWA canopy; thus, need to add minimum two slopes on the long sides and required linear feet.

Change

Sec. 5-707.D.6. Canopies

- A. Flat roof canopies are prohibited. Roofs shall have at least two slopes on the long sides and a roof detail change a minimum of every 50 feet.
- B. Canopies shall be consistent with the architectural design and features of the principal structure.
- C. Canopy lighting shall comply with Sec. 5-605.E, Canopy.

41. Comment

5-1302 A. Residential Impact Standards – Applicability – General: Neale Montgomery by letter requests clarity regarding applicability of the standards

Change

Sec. 5-3102. Applicability

A. General

Except as provided in subsection B below, standards of this section apply to all development applications orders and planned development rezonings that relate to the establishment, expansion, or intensification of a commercial use on land that:

SECTION 5-9: PLAT STANDARDS

42. Comment

Sec. 5-902 Applicability: Staff recommends the lists of exceptions for plat standards in 5-902 and 2-502(C) Plat Review be revised for consistency.

Change

5-902. Applicability

A. The standards of this section apply to all development in the Village, unless exempted in accordance with subsection B below. All development subject to the requirements of this section shall receive approval of a plat prior to the sale, conveyance, or transfer of land.

B. The following are exempt from the standards of this section.

- 1. A division of land that was accomplished in accordance with a development platted or approved by Lee County prior to January 28, 1983, provided that all required improvements are made, or that a security for the performance of the improvements has been posted and is current.
- 2. The division of land for the conveyance of land to a federal, state, county, or Village entity, or to a public utility.
- 3. The division of land by judicial decree.
- 4. A division of land of two or fewer lots out of a parent parcel approved in accordance with Sec. 2-502.B, Limited Development Order provided the division does not result in the creation of three or more lots out of a parent parcel that existed on January 28, 1983.

Chapter 6: SIGNAGE

43. Comment

Public comment requests change as follows:

6-104 Prohibited signs

B. Specific Prohibitions

30. Tri-fold, revolving, or multiple display changing signs or billboards

Change

Sec. 6-104.B. Specific Prohibitions

Specific types of signs that are prohibited within the Village include, without limitation, the following:

- Any sign that is not designed, located, constructed, or maintained in accordance with the provisions of this chapter, is not compatible with the objectives of this LDC, or does not meet the requirements of applicable Village, state, and federal codes.
- 2. Lights and signs that resemble any traffic control device, official traffic control signs, or emergency vehicle markings.

[...]

30. Tri-fold, revolving, or multiple display changing signs or billboards

Chapter 10: DEFINITIONS AND RULES FOR CONSTRUCTION, INTERPRETATION, AND MANAGEMENT

44. Comment

Staff recommended adding a definition of "multiple-occupancy complex" to accompany the parking standard

Change

Multiple-Occupancy Complex

For the purposes of Chapter 6: Signage, only, a A parcel of property under one ownership or singular control, or developed as a unified or coordinated project, with a building or buildings housing more than one at least five occupants conducting a business operation of any kind. For purposes of Chapter 6, Signage, only, a multiple-occupancy complex shall have more than one occupant.

45. Comment

Staff recommended adding a general definition of open space similar to the description in section 5-408.

Change

Open Space

For the purpose of bonus density calculations only, land owned by the Village for the use and enjoyment of the public and maintained with minimal buildings or improvements. For all other purposes, space on a lot without buildings that is kept in a natural state, landscaped, or available for outdoor activity, not including parking. Such spaces include native and indigenous preservation areas; outdoor active and passive public use areas such as plazas, atriums, courtyards, and other similar space; Buffers and vehicular use area landscaping; dry detention areas; bodies of water, including stormwater management areas and areas subject to saltwater inundation; active and passive recreation areas, such as playgrounds, golf courses, nature trails, bikeways, pedestrian ways, tennis courts, swimming pools, and other similar open spaces; and archaeological sites that are designated as significant historic resources; as limited by Sec. 5-408.D.

Appendix C: GENERAL ROAD SPECIFICATIONS

46. Comment

Table C-(A)1 – changes recommended by staff and for consistency with Appendix D

Change

TABLE C-(A)1: SPECIFICATIONS FOR VILLAGE-MAINTAINED ROADS						
Specification	Arterial Road	Collector Road	Local Road	Access Road		
	Standard Right-of-Way Widths (feet)					
Closed drainage	150 165	100 150	50 65	4 5 65		
Open drainage	150 200	100 145	60 80	50 <u>80</u>		
	Des	ign Speed (mp	oh)			
Rural Section						
With speed restriction	55 50	40-45	n/a	n/a		
Without speed restrictions	70	45-50	n/a	n/a		
Urban Section						
With speed restriction	35-40<u>45</u>	30-45	n/a	n/a		
Without speed restrictions	35-50	40-45	n/a	n/a		

47. Comment

Table C-(A)2 – changes recommended by staff and for consistency with Appendix D

Change

TABLE C-(A)2: SPECIFICATIONS FOR PRIVATELY-MAINTAINED ROADS					
Specification	Local Road Access Road				
Minimum Right-of-Way/Easement Widths (feet)					
One-way					
Closed drainage, rear-lot drainage or inverted crown	30	30			
Open Drainage	40	35			

TABLE C-(A)2: SPECIFICATIONS FOR PRIVATELY-MAINTAINED ROADS					
Specification	Local Road	Access Road			
Two-way					
Closed drainage or inverted crown	40 <u>60</u>	40 <u>60</u>			
Open drainage	4 <u>5</u> 60	40 <u>60</u>			
Minimum Grade of Roads (Profile)					
Closed drainage	0.2 0.3%	0.2 <u>0.3%</u>			
Inverted Crown	0.4 1.0%	0.4 1.0%			
Open drainage	0.0%	0.0%			
Additional Minimum (feet)					
Minimum centerline radius for horizontal curves	50	50			

B(7)A.2 – changes recommended by staff and for consistency with Appendix D

Change

- 7. Cul-de-Sacs
- A. A road intended to be a permanent dead-end road shall be closed at one end by a circular turnaround for vehicles constructed to the following standards:
 - 1. The diameter of pavement to inside edge of curb or edge of pavement shall be a minimum of 90 feet outside diameter, and a maximum of 45 feet inside diameter;
 - 2. The diameter of right-of-way for the curb-and-gutter section shall be 110 130 feet; and
 - 3. The diameter of right-of-way for ditch and swale drainage shall be a minimum of 130 feet.

49. Comment

B(14) – changes recommended by staff and for consistency with Appendix D

Change

14. Road and Bridge Development Categories

For purposes of interpreting the specifications contained in this section and Appendix D: Illustrations and Cross-Sections, development categories are defined as follows:

A. Category A

Category A includes commercial and industrial developments and all developments not described in Categories B, C, or D.

1. Pavement width, one way traffic

14-foot pavement for one-way traffic with swale drainage or valley gutter drainage, or 16-foot pavements for one-way traffic with curb and gutter drainage.

2. Pavement width, two-way traffic

24-foot pavements for two-way traffic with swale drainage, valley gutter drainage or curb and gutter drainage (27 feet minimum from face of curb to face of curb on nonmountable curbs.) See Appendix D: Illustrations and Cross-Sections, Section D.

* * *

B. Category B

Category B includes residential developments of five or more dwelling units per acre.

1. Pavement width, one way traffic

14-foot pavement for one-way traffic with swale drainage or valley gutter drainage, or 16-foot pavements for one-way traffic with curb and gutter drainage. (19 feet minimum from face of curb to face of curb on nonmountable curbs.)

2. Pavement width, two-way traffic

20-foot pavements for two-way traffic with swale drainage or valley gutter drainage or 24-foot pavement with curb and gutter drainage (27 feet minimum from face of curb to face of curb on nonmountable curbs.) See Appendix D: Illustrations and Cross-Sections, Section D.

* * *

C. Category C

Category C includes all residential roads

1. Pavement width, one way traffic

14-foot pavement for one-way traffic with swale drainage or valley gutter drainage, or 16-foot pavements for one-way traffic with curb and gutter drainage. (19 feet minimum from face of curb to face of curb on nonmountable curbs.)

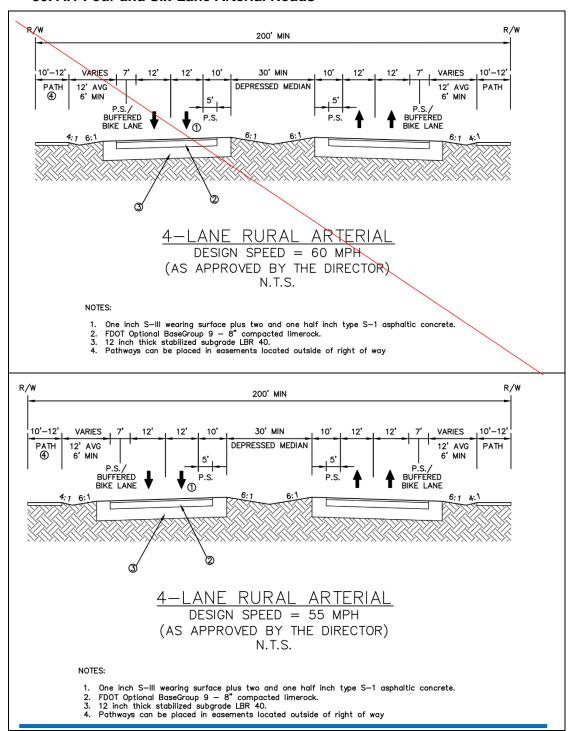
2. Pavement width, two-way traffic

20-foot pavements for two-way traffic with swale drainage or valley gutter drainage or 24-foot pavement with curb and gutter drainage (27 feet minimum from face of curb to face of curb on nonmountable curbs.)

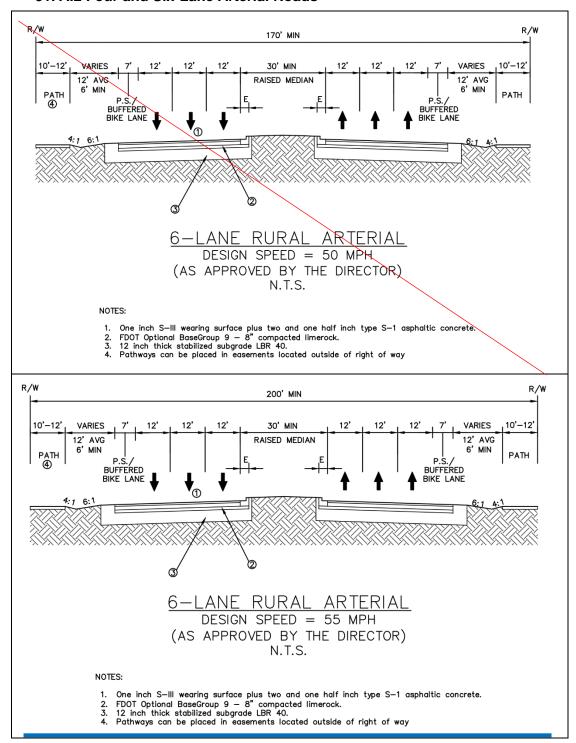
Appendix D: ILLUSTRATIONS AND CROSS SECTIONS

Changes to Appendix D on the following pages are technical in nature and recommended by Village staff

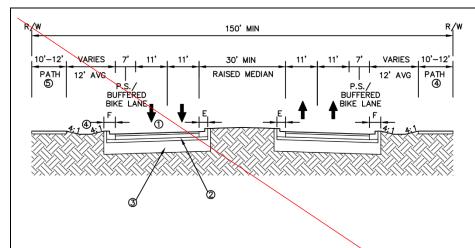
50. A.1 Four and Six-Lane Arterial Roads



51. A.2 Four and Six-Lane Arterial Roads



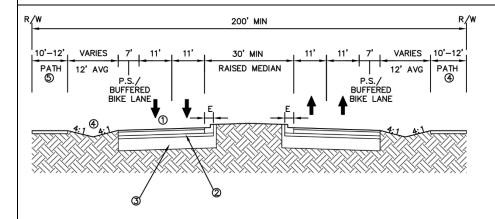
52. A.3 Four and Six-Lane Arterial Roads



4-LANE SUBURBAN ARTERIA DESIGN SPEED = 45 MPH (AS APPROVED BY THE DIRECTOR) N.T.S.

NOTES:

- One inch S-III wearing surface plus two and one half inch type S-1 asphaltic concrete.
 FDOT Optional BaseGroup 9 8" compacted limerock.
 12 inch thick stabilized subgrade LBR 40.
 This size open drainage ditches are insufficient in size to retain all stormwater. Off-site retention ponds or additional drainage easements may be required.
 Pathways can be placed in easements located outside of right of way.



4-LANE SUBURBAN ARTERIAL DESIGN SPEED = 45 MPH (AS APPROVED BY THE DIRECTOR) N.T.S.

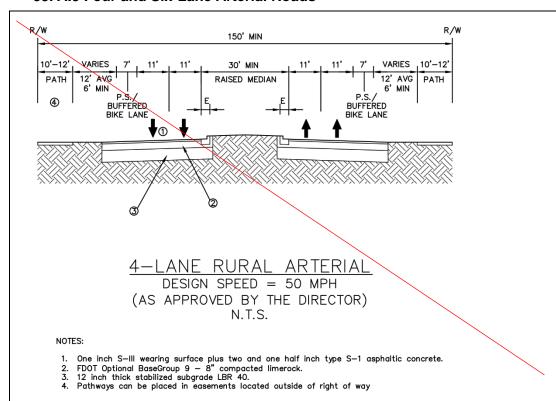
- One inch S-III wearing surface plus two and one half inch type S-1 asphaltic concrete.
- FDOT Optional BaseGroup 9 8" compacted limerock.

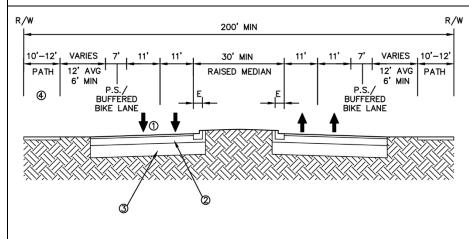
 12 inch thick stabilized subgrade LBR 40.

 This size open drainage ditches are insufficient in size to retain all stormwater. Off—site retention ponds or additional drainage easements may be required.

 Pathways can be placed in easements located outside of right of way.

53. A.5 Four and Six-Lane Arterial Roads



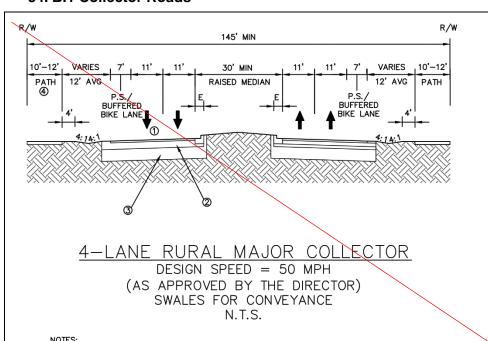


4-LANE RURAL ARTERIAL DESIGN SPEED = 50 MPH (AS APPROVED BY THE DIRECTOR) N.T.S.

- One inch S-III wearing surface plus two and one half inch type S-1 asphaltic concrete. FDOT Optional BaseGroup 9 8" compacted limerock.

 12 inch thick stabilized subgrade LBR 40.
 Pathways can be placed in easements located outside of right of way

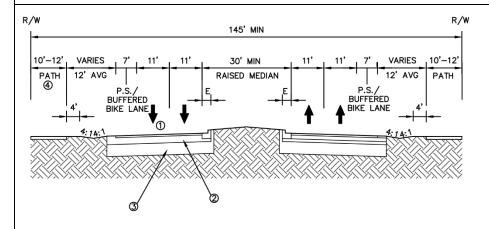
54. B.1 Collector Roads



NOTES:

- One and one half inch S-I plus one inch type S-III asphaltic concrete.

- Eight inch compacted limerock (optional basegroup 9).
 12 inch thick stabilized subgrade LBR 40.
 Pathways can be placed in easements located outside of right of way.

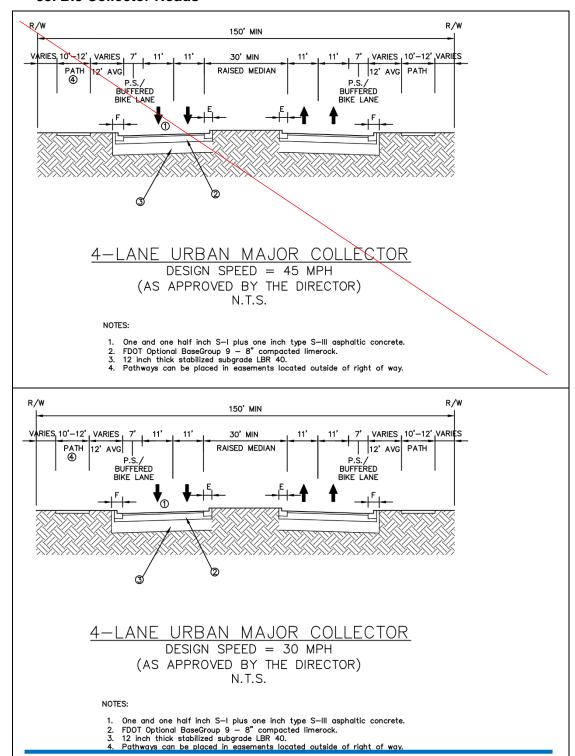


4-LANE RURAL MAJOR COLLECTOR

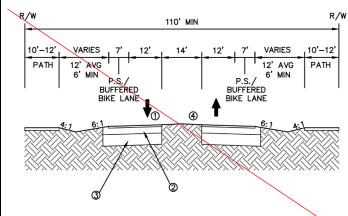
DESIGN SPEED = 40-45 MPH (AS APPROVED BY THE DIRECTOR) SWALES FOR CONVEYANCE N.T.S.

- One and one half inch S—I plus one inch type S—III asphaltic concrete. Eight inch compacted limerock (optional basegroup 9).
 12 inch thick stabilized subgrade LBR 40.
 Pathways can be placed in easements located outside of right of way.

55. B.3 Collector Roads



56. B.4 Collector Roads

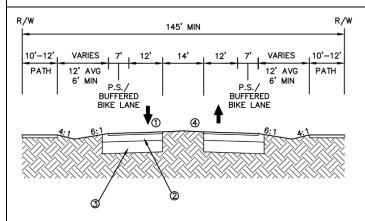


3-LANE RURAL MAJOR COLLECTOR

DESIGN SPEED = 45 MPH (AS APPROVED BY THE DIRECTOR) SWALES FOR CONVEYANCE N.T.S.

NOTES:

- One and one half inch S-I plus one inch type S-III asphaltic concrete.
 FDOT Optional BaseGroup 9 8" compacted limerock.
 12 inch thick stabilized subgrade LBR 40.
 A 14 foot two-way left turn lane may be considered subject to approval by the Public Works Director



3-LANE RURAL MAJOR COLLECTOR

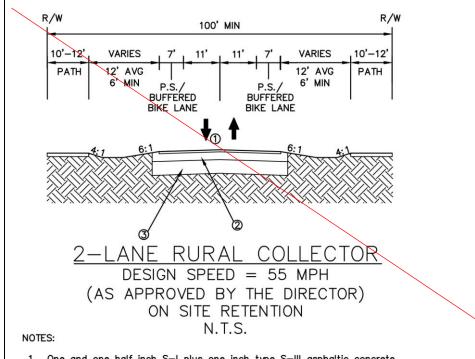
DESIGN SPEED = 40 MPH (AS APPROVED BY THE DIRECTOR) SWALES FOR CONVEYANCE N.T.S.

OTES:

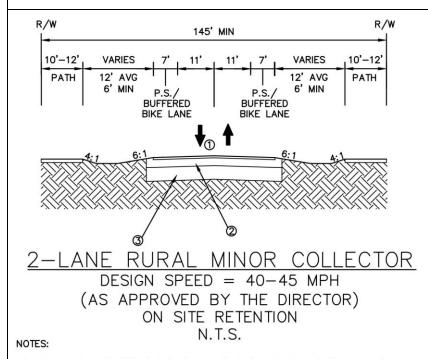
- 2. 3. 4.
- One and one half inch S—I plus one inch type S—III asphaltic concrete. FDOT Optional BaseGroup 9 8" compacted limerock.
 12 inch thick stabilized subgrade LBR 40.
 A 14 foot two—way left turn lane may be considered subject to approval by the Public Works Director

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57. B.5 Collector Roads

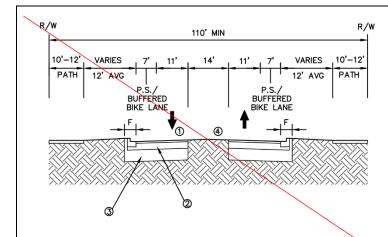


- One and one half inch S-I plus one inch type S-III asphaltic concrete.
 FDOT Optional BaseGroup 9 8" compacted limerock.
 12 inch thick stabilized subgrade LBR 40.



- 1. One and one half inch S-I plus one inch type S-III asphaltic concrete.
- FDOT Optional BaseGroup 9 8" compacted limerock. 12 inch thick stabilized subgrade LBR 40.

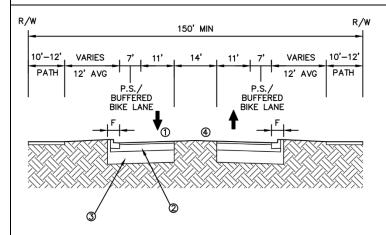
58. B.6 Collector Roads



3-LANE URBAN COLLECTOR DESIGN SPEED = 45 MPH (AS APPROVED BY THE DIRECTOR) N.T.S.

NOTES:

- One and one half inch S-I plus one inch type S-III asphaltic concrete.
 FDOT Optional BaseGroup 9 8" compacted limerock.
 12 inch thick stabilized subgrade LBR 40.
 A 14 foot two-way left turn lane may be considered subject to approval by the Public Works Director



3-LANE URBAN MINOR COLLECTOR

DESIGN SPEED = 30-45 MPH (AS APPROVED BY THE DIRECTOR) N.T.S.

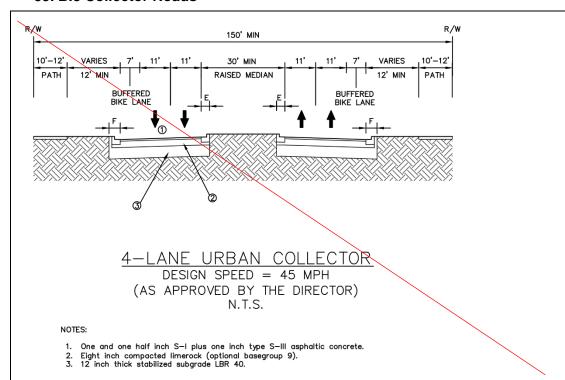
- One and one half inch S-I plus one inch type S-III asphaltic concrete.

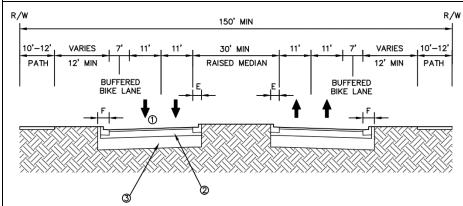
 FDOT Optional BaseGroup 9 8" compacted limerock.

 12 inch thick stabilized subgrade LBR 40.

 A 14 foot two-way left turn lane may be considered subject to approval by the Public Works Director

59. B.8 Collector Roads

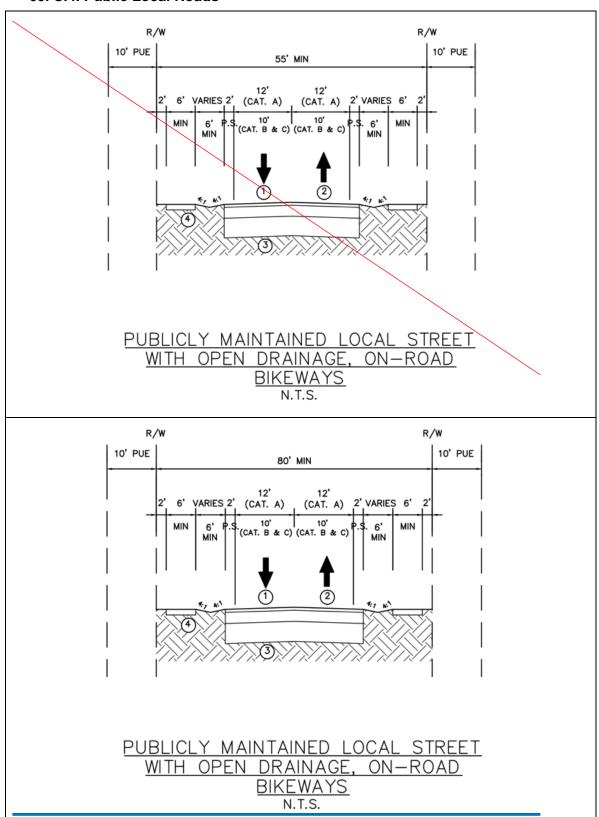




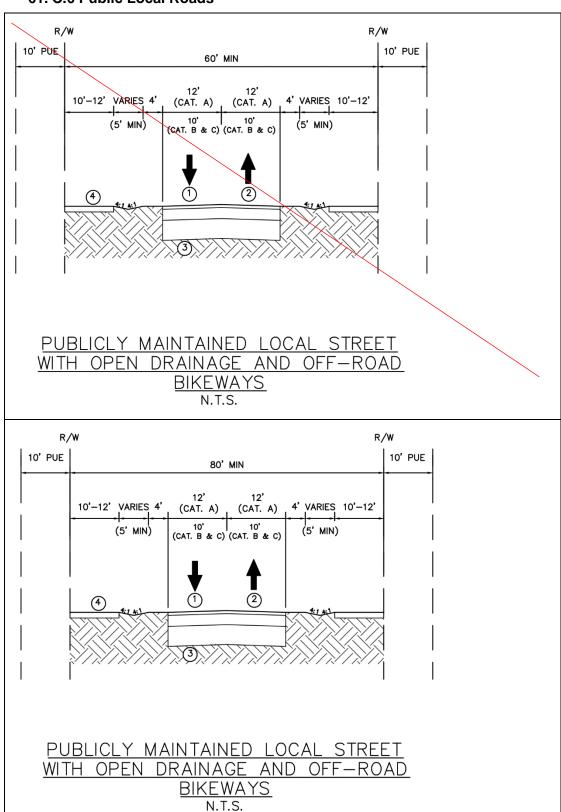
4-LANE URBAN MINOR COLLECTOR DESIGN SPEED = 30-45 MPH (AS APPROVED BY THE DIRECTOR) N.T.S.

- One and one half inch S-I plus one inch type S-III asphaltic concrete.
- Eight inch compacted limerock (optional basegroup 9). 12 inch thick stabilized subgrade LBR 40.

60. C.4. Public Local Roads



61. C.6 Public Local Roads

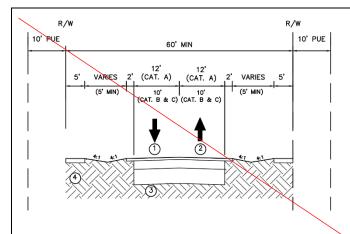


62. D.1 Private Local Roads

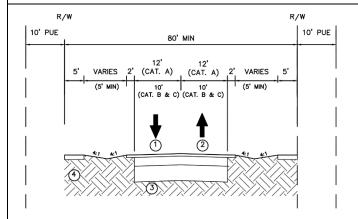
Note	Category B & C	Category A
1	1" Type S-III asphalt concrete [2]	1½" Type S-IH asphalt concrete
2	6" Base	8" Base
3	6" Stabilized subgrade	12" Stabilized subgrade
4	Sidewalk — required on one side	Sidewalk — required on one side

^[1] A ten-foot-wide public utility easement shall be provided abutting each side of the right-of-way.

63. D.2 Private Local Roads



PRIVATELY MAINTAINED LOCAL STREET WITH OPEN DRAINAGE N.T.S.



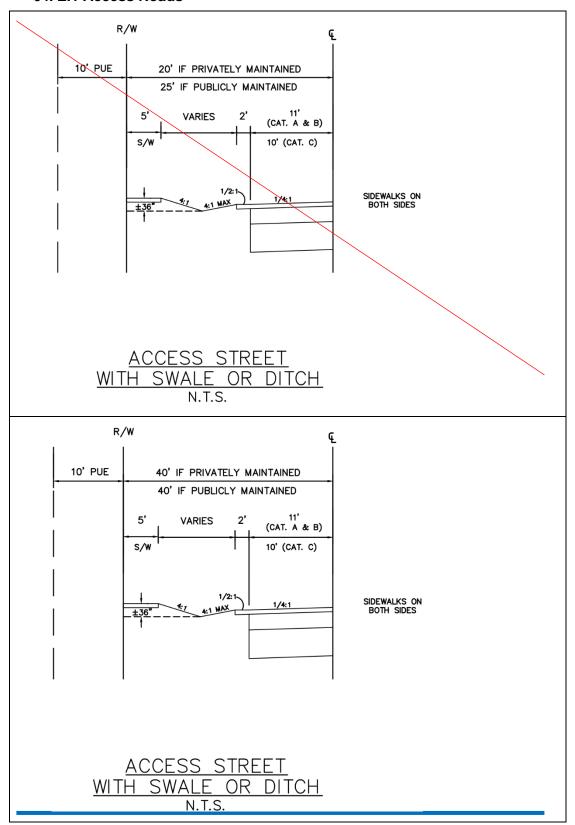
PRIVATELY MAINTAINED LOCAL STREET WITH OPEN DRAINAGE N.T.S.

Note	Category B & C	Category A
1	1" Type S-III asphalt concrete [2]	1½" Type S-IH asphalt concrete
2	6" Base	8" Base
3	6" Stabilized subgrade	12" Stabilized subgrade
4	Sidewalk — required on one side	Sidewalk — required on one side

NOTES:

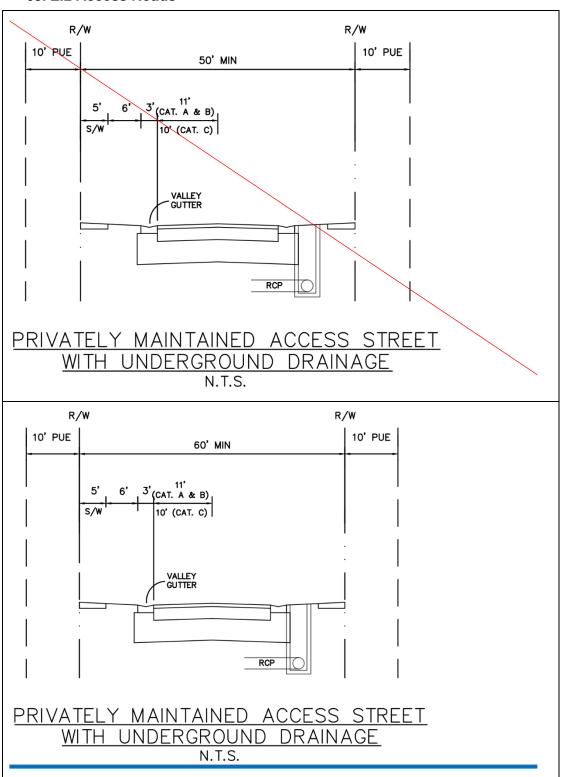
[1] A ten-foot-wide public utility easement shall be provided abutting each side of the right-of-way.

64. E.1 Access Roads



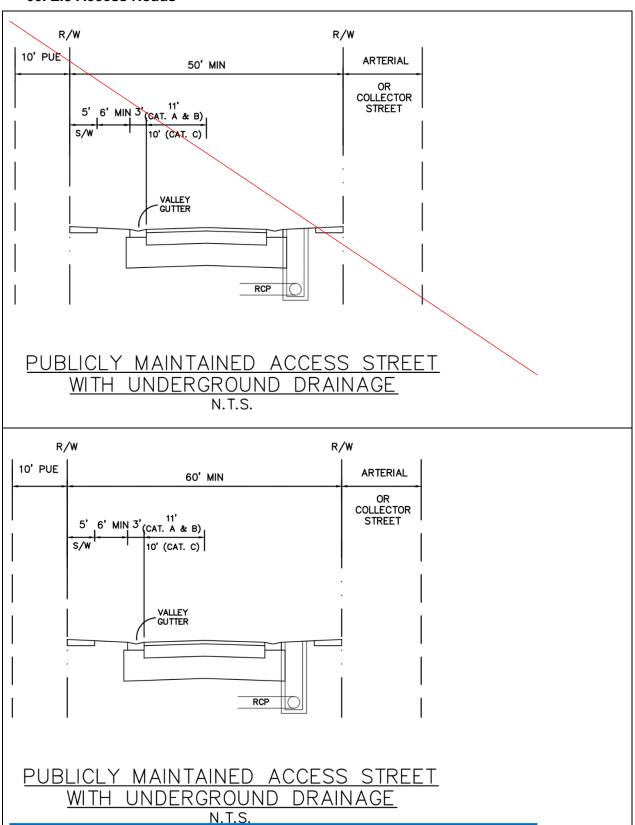
50

65. E.2 Access Roads



51

66. E.3 Access Roads



Category	Minimum Pavement Width (feet)	Asphaltic Concrete Surface Course	Base (ft)	Stabilized Subgrade LBR 40 (ft)
Α	22	1½" Type S-I or S-III	8	12
В	22	1½" Type S-III or S-III	6	6
С	20	1½" Type S-III or S-III	6	6

Appendix F: RECOMMENDED PLANT LISTS

67. Comment

Multiple comments were provided on Appendix F: Recommended Plant Lists, recommended changes to the list of recommended species, concern about the pictures use to illustrate permitted plants and the absence of pictures for some, and disagreement whether certain species was native or non-native. Based upon the number of comments, we recommend not including the plant palette as a comprehensive list within the LDC. Instead, the plant list will be a guide for staff and designers to use.

Changes

Sec. 5-403.A Plant Suitability

Plant materials shall be suited to the climate and suited for their planting location at maturity.

- 1. Required plantings shall be in accordance with the Estero plant palette, which is set out in Appendix F: Recommended Plant Lists.
- 2. The invasive exotic plants identified in Table 5-403.A.2: Prohibited Invasive Exotic Plants, are prohibited and shall be removed. Methods to remove and control invasive exotic plants shall be included in development orders or limited development orders, as applicable. A statement shall also be included in development orders or limited development orders that the development area will be maintained free from invasive exotic plants in perpetuity.

TABLE5-403.A.2: PROHIBITED INVASIVE EXOTIC PLANTS					
Common	Scientific name	Common	Scientific name		
name		name			
Earleaf acacia	Acacia	Old World	Lygodium		
	auriculiformis	climbing fern	microphyllum		
[]			·		

Sec. 5-403.B.2. Native varieties

At least 75 percent of the trees and 50 percent of the shrubs used to comply with the requirements of this section shall be native Florida species, identified in Appendix F: Recommended Plant Lists.

Sec. 7-206.G.3.C.1.(b)

The planting of native shade trees, meeting the specifications of Appendix F: Recommended Plant Lists, around the lake perimeter, calculated at one tree per 100 feet of lake shoreline measured at control elevation. The tree planting is in addition to other required trees and shall be coordinated with lake littoral plant requirements. The planting locations proposed to meet the wetland herbaceous plant requirements set forth in Sec. 5-408.I.5, Planted Littoral Shelf (PLS), and other additional trees, shall be graphically identified as part of the deep lake management plan. All required plantings shall be grouped or clustered together around the lake perimeter.

Sec. 10-4 Definitions:

Indigenous Native Vegetation

Plant species that are characteristic of the major plant communities of the area, listed as native in Appendix F: Recommended Plant Lists. Areas where invasive exotic vegetation (see Invasive Exotic Vegetation) has exceeded 75 percent of the plant species by quantity will not be considered indigenous vegetation.

Appendix F: Recommended Plant Lists